

ESTABLISHING  
FUNDAMENTAL  
PLANNING,  
DEVELOPMENT, LAND  
USE CONCEPTS,  
AND URBAN DESIGN  
STANDARDS TO  
GUIDE REDEVELOPMENT  
THROUGH THE  
CENTENNIAL AND  
BEYOND

A stylized, semi-transparent map of downtown Las Vegas is overlaid on the background. The map shows a grid of streets and the outlines of major buildings, including the New York-New York Hotel & Casino and the Flamingo Las Vegas. The map is tilted slightly to the right.

# LAS VEGAS DOWNTOWN CENTENNIAL

plan

# LAS VEGAS DOWNTOWN CENTENNIAL plan

The City of Las Vegas Downtown Centennial Plan was

adopted by CCDC February 17, 2000

adopted by Planning Commission April 27, 2000

adopted by City Council July 5, 2000

updated:

Ordinance #5550 December 4, 2002

Ordinance #5566 February 5, 2003

General Plan Amendment #GPA-2591 August 13, 2003

General Plan Amendment #GPA-3000 November 5, 2003

General Plan Amendment #GPA-3130 November 5, 2003



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ORIGINAL ADOPTION DATE: JUNE 5, 2000

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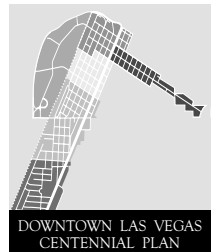
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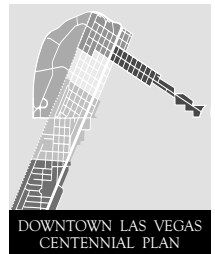
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# TABLE OF CONTENTS

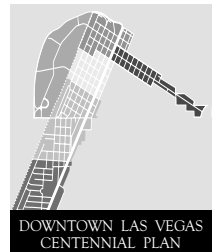
V. VISION OF DOWNTOWN LAS VEGAS CENTENNIAL.....	1
V1. DOWNTOWN RENAISSANCE.....	3
V1A. Brief History of Downtown .....	11
V2. POLICY CONTEXT FOR DOWNTOWN LAS VEGAS.....	14
V2A. Area of Study .....	14
V2B. Background of Previous Planning Efforts .....	17
V2C. Applicability of this Plan .....	19
V3. URBAN DESIGN FRAMEWORK .....	20
V3A. Gateways.....	20
V3B. Anchors.....	21
V3C. Spine .....	22
V3D. Linkages .....	23
V3E. Proposed Mixed Use Opportunities.....	26
V4. DEFINITION OF DOWNTOWN DISTRICTS .....	31
DS. DOWNTOWN LAS VEGAS DESIGN STANDARDS .....	
DS1. URBAN DESIGN GOALS & OBJECTIVES.....	34
DS2. SITE PLANNING STANDARDS .....	36
DS3. TRANSPORTATION AND PARKING STANDARDS.....	37
DS4. STREETScape DESIGN STANDARDS .....	40
DS5. ARCHITECTURAL DESIGN STANDARDS .....	44
DS6. SIGNAGE DESIGN STANDARDS.....	46
DS7. LANDSCAPE DESIGN STANDARDS.....	46
DS8. DOWNTOWN LAS VEGAS OUTDOOR DINING STANDARDS .....	46
OC. OFFICE CORE DISTRICT: VISION AND DESIGN STANDARDS .....	51
V5. OFFICE CORE: BOUNDARIES .....	51
V5A. OFFICE CORE: Existing Conditions.....	52
V5B. OFFICE CORE: Market Demand .....	52
V5C. OFFICE CORE: Land Use / Urban Design Constraints .....	53
V5D. OFFICE CORE: Transportation and Parking Needs .....	55
V5E. OFFICE CORE: Strategies.....	56
V 5E1. OFFICE CORE: Market Strategies.....	56
V 5E2. OFFICE CORE: Land Use and Urban Design Strategies .....	56
V 5E3. OFFICE CORE: Transportation Strategies.....	56





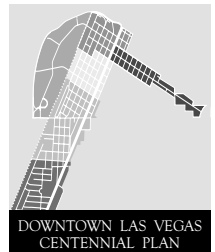
# TABLE OF CONTENTS CONTINUED

V6. OFFICE CORE: URBAN DESIGN PRINCIPLES .....	57
V6A. OFFICE CORE: Urban Form .....	57
V6B. OFFICE CORE: Potential Redevelopment Areas.....	58
V6C. OFFICE CORE: Zoning .....	59
V6D. OFFICE CORE: Vehicular Transportation / Access .....	59
V6E. OFFICE CORE: Primary Pedestrian System .....	61
V6F. OFFICE CORE: Parcel Build-To Lines and Streetwalls .....	61
V6G. OFFICE CORE: Ground Level Uses .....	62
 OC.OFFICE CORE DISTRICT DESIGN STANDARDS	
OC1. OFFICE CORE: Site Planning Standards.....	66
OC2. OFFICE CORE: Transportation and Parking Standards.....	68
OC3. OFFICE CORE: Streetscape Design Standards .....	70
OC4. OFFICE CORE: Architectural Design Standards .....	71
OC5. OFFICE CORE: Signage Design Standards.....	72
OC6. OFFICE CORE: Landscape Design Standards .....	73
OC7. OFFICE CORE: Arcade Design Standards.....	73
 AD.ARTS DISTRICT DESIGN STANDARDS .....	75
AD1. ARTS DISTRICT: Site Planning Standards .....	76
AD2. ARTS DISTRICT: Transportation and Parking Standards .....	78
AD3. ARTS DISTRICT: Streetscape Design Standards.....	79
AD4. ARTS DISTRICT: Architectural Design Standards .....	80
AD5. ARTS DISTRICT: Signage Design Standards .....	81
AD6. ARTS DISTRICT: Landscape Design Standards .....	82
AD7. ARTS DISTRICT: Transit Corridor Design Standards .....	82
 PART 2: PARKWAY CENTER PROPOSED DEVELOPMENT STANDARDS	
Introduction .....	83
Proposed Development Standards .....	89
Site Planning and Physical Design .....	95
Pedestrian Orientation/Streetscape .....	97
Parking, Transportation and Vehicular Access .....	100
Architectural Design.....	103
Landscape Architecture Design.....	105
Signage Design and Placement.....	110
Lighting.....	113



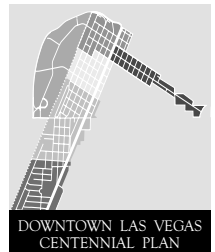
# TABLE OF CONTENTS: MAPS

Map 1:	DOWNTOWN: Recent Developments.....	4
Map 2:	DOWNTOWN: Current Developments.....	6
Map 3:	DOWNTOWN: Historic Block and Parcel Pattern.....	11
Map 4:	DOWNTOWN: As Hub of Las Vegas Valley .....	12
Map 5:	Las Vegas Downtown Centennial Plan Boundary.....	15
Map 6:	Seven Downtown Districts.....	16
Map 7:	Urban Trail Network.....	27
Map 8:	OFFICE CORE: District Boundary .....	51
Map 9:	OFFICE CORE: Existing Urban Form Plan.....	53
Map 10:	OFFICE CORE: Potential Redevelopment Areas Plan.....	58
Map 11:	OFFICE CORE: Transportation / Access Plan .....	60
Map 12:	OFFICE CORE: Primary Pedestrian System Plan .....	63
Map 13:	OFFICE CORE: Streetwall and Parcel Build-To Plan.....	64
Map 14:	OFFICE CORE: Ground Floor Special Use Plan.....	65
Map 15:	ARTS DISTRICT: District Boundary.....	75



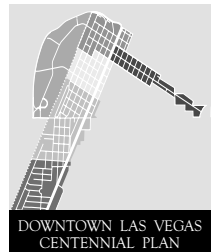
# TABLE OF CONTENTS: GRAPHICS

Graphic 1:	Regional Justice Center Rendering .....	8
Graphic 2:	Historic Parcel Configuration.....	12
Graphic 3:	Fixed Guideway Station.....	25
Graphic 4:	Alleyway Treatment.....	36
Graphic 5:	Parking Lot Design Standards .....	38
Graphic 6:	Parking Structure with Arcade and Cafes .....	39
Graphic 7:	Typical North-South Streets.....	42
Graphic 8:	Typical East-West Streets .....	43
Graphic 9:	Building Facade at Street Level.....	44
Graphic 10:	Building Massing and Setbacks .....	45
Graphic 11:	Outdoor Dining Location .....	48
Graphic 12:	Outdoor Dining Barriers.....	48
Graphic 13:	Recommended Arcade Envelope Zone .....	74



# TABLE OF CONTENTS: PHOTOS

Photo 1:	Aerial of Downtown .....	1
Photo 2:	Fremont Street Experience .....	3
Photo 3:	Clark County Government Center .....	3
Photo 4:	Fourth Street Corridor Improvements .....	3
Photo 5:	Downtown Gateway Feature 1998 .....	5
Photo 6:	Federal Building / U.S. Courthouse .....	7
Photo 7:	Intown Office .....	7
Photo 8:	Lewis Avenue Corridor Pedestrian Walkway.....	7
Photo 9:	Neonopolis at Fremont Street Experience.....	8
Photo 10:	Arts Factory Building .....	10
Photo 11:	Fremont Street 1911 .....	13
Photo 12:	Fremont Street 1958.....	13
Photo 13:	Fremont Street Experience Plaza.....	21
Photo 14:	Las Vegas Boulevard Landscape Improvements.....	22



## V. VISION OF DOWNTOWN LAS VEGAS CENTENNIAL

1 Las Vegas will celebrate the 100th anniversary of its founding in 2005 and its incorporation as a City in 2011. The Downtown Las Vegas Centennial Plan (hereinafter “the Plan”) establishes fundamental planning, development, and land use concepts, and especially urban design standards to guide the redevelopment of the original core of the burgeoning Las Vegas Valley through its centennial and beyond. This plan is a component of the City’s new 2020 Master Plan, and therefore, shares a 20 year horizon for full implementation. Several major projects are currently in development that will begin to change the face, and the public’s impressions, of downtown. These projects will greatly improve the tax base for the City—which will in turn enhance future redevelopment opportunities. Jobs will be created— construction and permanent, professional and service employment. The critical mass of several thousand new workers and some 12,000 additional daily visitors to downtown will sustain a variety of new

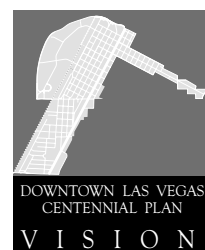
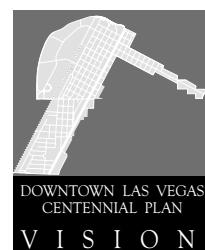


PHOTO 1: AERIAL OF DOWNTOWN LAS VEGAS



cafes, restaurants, and supporting commercial uses. Within three to five years, tremendous opportunities for new development will emerge in and around downtown, from high-rise condominiums with premium valley-wide views to cultural centers. This growing synergy of uses will create a dynamic, rich, urbane environment that will promote other redevelopment and desirable land uses downtown. This will create a complex and intriguing pedestrian-oriented 24-hour lifestyle.

**2** This Plan sets forth a long-term strategy to re-establish Downtown Las Vegas as the region's premier cultural, civic, financial, and business center. Downtown is becoming a destination that tourists, residents, and business people will seek out for its employment and residential opportunities, ease of access, and unique image. It will have high quality office complexes, restaurants catering to both business and evening activity patrons, parks, plazas, streets lined with trees and arcades, hotels, the "historic" casinos, entertainment, shopping, museums, and perhaps most importantly, new residential communities and neighborhoods. It remains the home to Las Vegas City Hall, the Clark County Government Seat, regional courts, federal and civic uses. The resurgence of activity will generate economic vitality and make Downtown Las Vegas a safe and pleasant place to live, work, and play.



**3** The Plan's vision section provides details on all the new development projects springing up downtown, describes the history of downtown, defines the area of this particular study, and explains the history of previous planning efforts that have led up to this new Plan.

## V1. DOWNTOWN RENAISSANCE

3

**1** Downtown Las Vegas is beginning its first renaissance as the true urban center for the entire Las Vegas Valley. The opening of the Fremont Street Experience in 1995 (Photo 2) marked the beginning of a series of new development projects and a succession of rapid, positive changes. The new Clark County Government Center (Photo 3) was dedicated in 1997, anchoring the initial redevelopment of the old Union Pacific rail yards and setting high architectural standards for all development to follow.

**2** The City of Las Vegas invested 25 million dollars in public infrastructure between 1996 and 1998. The Fourth Street Corridor (Photo 4), lined with palm trees and

PHOTO 2: FREMONT STREET EXPERIENCE

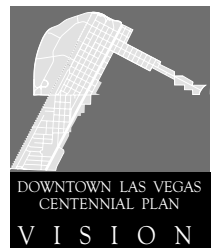
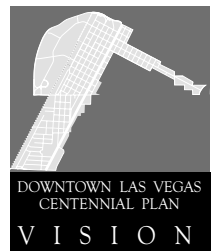
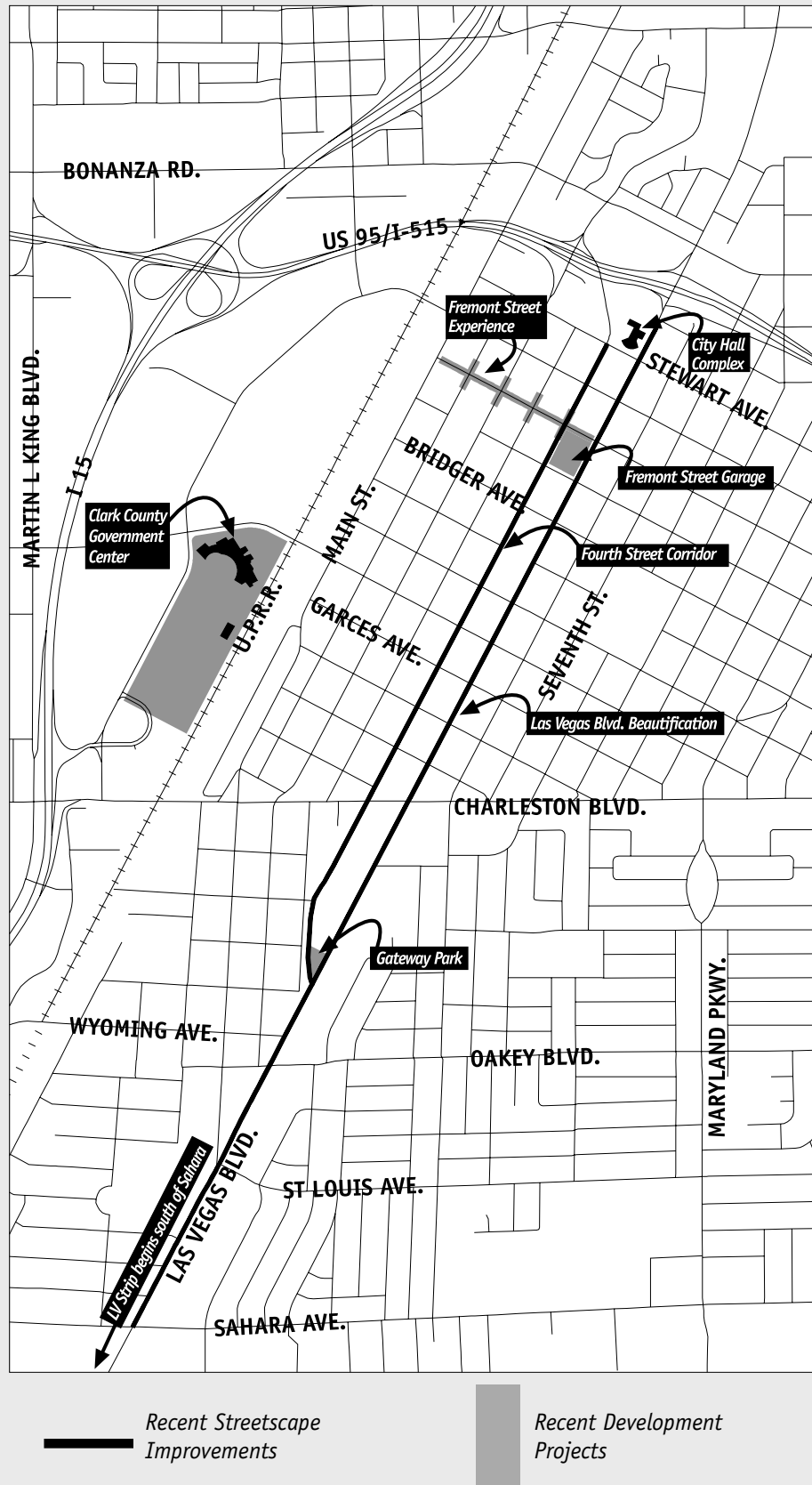


PHOTO 3: CLARK COUNTY GOVERNMENT CENTER



PHOTO 4: FOURTH STREET CORRIDOR IMPROVEMENTS





MAP 1: DOWNTOWN: RECENT DEVELOPMENTS



wide sidewalks, has been completely rebuilt from Las Vegas Boulevard to Interstate 515. The Downtown Gateway Landscape Feature (Photo 5), with its lush canopy of 222 palm trees, marks the arrival of downtown and anchors the southern realms of downtown. Las Vegas Boulevard has been beautified with a landscaped median from one end of downtown to the other, providing visitors with seamless visual continuity from the Strip to City Hall (Map 1). The City continues to plan for the future and will support private development with additional right-of-way and streetscape improvements. Two additional projects are now being discussed: the Lewis Avenue Corridor which will create the central spine for the Office Core District, and the Third Street Pedestrian Connector which will greatly enhance foot traffic between the Fremont Street Experience and the Lewis Avenue Corridor and later southward into new residential neighborhoods and the Arts District (Map 2).

3 Currently, almost one billion dollars (\$1,000,000,000.) is being invested in construction projects in Downtown Las Vegas (Map 2):

- a. Federal Building / United States Courthouse broke ground in April of 1998 was completed in the Spring of 2000. It contains some 407,000 square feet of federal court rooms



PHOTO 5: DOWNTOWN GATEWAY





and offices and represents the investment of more than 90 million dollars in federal funds (Photo 6).

- b. Intown Office, The Intown Office, L.L.C., with the Pauls Corporation of Denver, developed a 110,000 square foot Class A office building downtown in cooperation with City Center Development Corporation and the Downtown Redevelopment Agency. The building is located at Fourth and Lewis, between the Regional Justice Center and the Fifth Street School complex. The Pauls tower will be integrated with a public parking garage and the City's improvement project on Lewis Avenue. Ground floor retail and commercial will contribute vitality to the Lewis Avenue project's pedestrian and landscape beautification (Photo 8).

PHOTO 6: FEDERAL BUILDING / U.S. COURTHOUSE

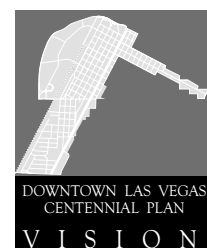


PHOTO 7: INTOWN OFFICE BUILDING



PHOTO 8: LEWIS AVENUE CORRIDOR PEDESTRIAN WALKWAY





Rendering by Tate & Snyder Architects

- c. Clark County Regional Justice Center complex occupies two urban blocks and marks an unprecedented cooperative arrangement between the City of Las Vegas and Clark County. This 18-story tower is scheduled for completion in summer 2004 and will contain 700,000 square feet of courts, public, and support space, including the District Attorney's offices. This 132 million dollar investment includes the City's commitment of 32 million dollars to relocate its Municipal Courts (Graphic 1).
- d. Clark County Detention Center Expansion was completed in October 2002 at a cost of \$78 million dollars.
- e. Neonopolis, an urban entertainment destination center, anchors the Las Vegas Boulevard end of the Fremont Street Experience and provides an exciting venue along Glitter Gulch for shopping, dining and catching a movie. This 100 million dollar project is a joint venture of the City and World Entertainment Centers / Las Vegas, Inc. (Prudential Insurance Company of America). The City assisted with land

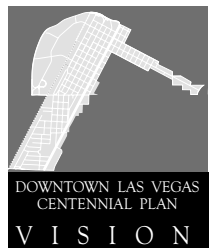
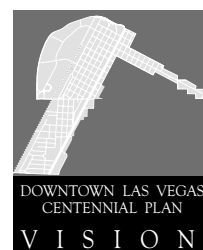


PHOTO 9: NEONOPOLIS AT FREMONT STREET EXPERIENCE



acquisition and retains ownership of a 600 space subterranean public parking garage (Photo 9).

- f. Resort Corridor Fixed Guideway System, Phase Two, will provide public transportation linking Fremont Street and the Las Vegas Strip. This 380 million dollar project of the Regional Transportation Commission is partially funded by 155 million dollars in federal funds and will likely open by 2005. The second phase may complete a loop system around the Strip and on to McCarran International Airport. Future phases could link suburban areas such as Centennial Hills Town Center to the northwest, Summerlin to the west, Henderson to the southeast and North Las Vegas to Fremont Street (Map 10).
- g. City Hall Campus expansion and renovation is proposed in four phases over the next 20 years and represents a commitment of \$43 million by the City of Las Vegas.
- h. Historic Fifth Street School re-use scenarios are currently being explored by the City and City Centre Development Corporation and could include community and cultural uses, a festival marketplace, and other supportive uses such as housing.
- i. Downtown Beautification Projects by the City total more than 25 million dollars.



4 These projects will create a new critical mass of people which will create further market demands for additional cafes, retail, and entertainment venues, thus transforming Downtown Las Vegas into a sophisticated and urbane environment. The redevelopment of the Arts Factory building (Photo 10) and the creation of other galleries and cafes in the Downtown South District have made it feasible to support the concept of creating an Arts District downtown with live-work and smaller-scaled redevelopment opportunities.

5 Within the next few years, the development of new residential communities will not only be possible, but highly desirable in rounding out the land uses downtown while creating an around-the-clock atmosphere. New residential towers with panoramic valley views, townhouses, and courtyard apartments will begin to rise on under-utilized parcels, and live-work lofts will be converted out of older office structures. Within the next few years, the fate of the vacant railyards may be determined with new buildings rising on this now vacant site readied for development.

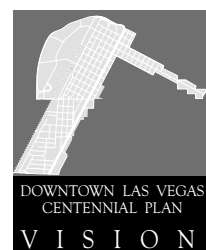
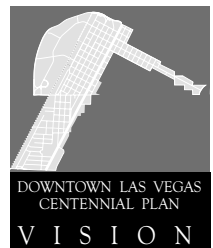


PHOTO 10: ARTS FACTORY BUILDING, A COLLECTION OF STUDIOS AND GALLERIES IN THE HEART OF THE ARTS DISTRICT

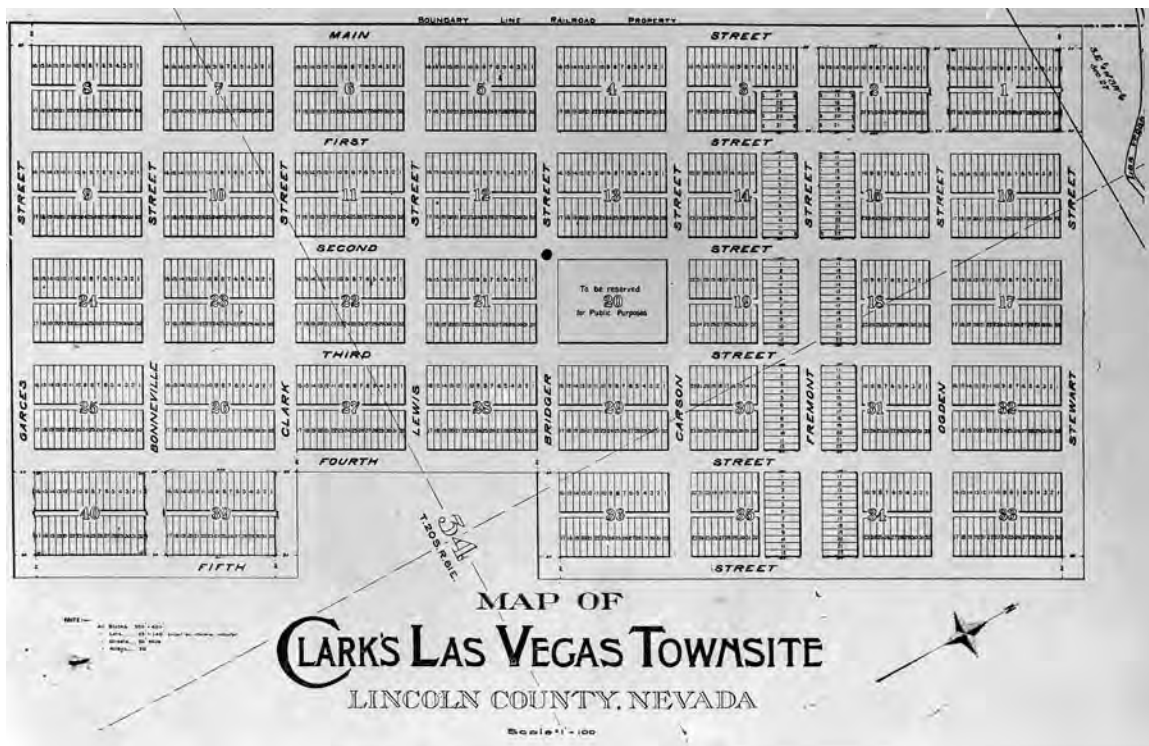


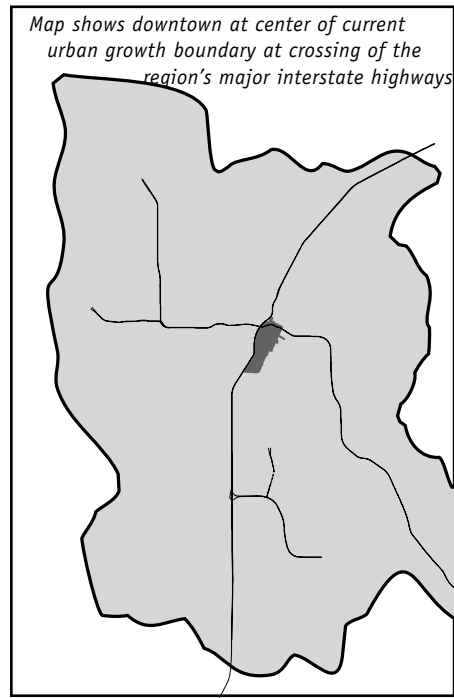
1 Las Vegas was founded by the Union-Pacific Railroad on May 5, 1905, as a commercial real estate venture. The railroad's realtor laid out an original grid of 40 blocks, each 300 by 400 feet, long-wise along the train tracks, and sold off lots on those blocks at auction. A 20 foot wide alley broke most blocks in half, and the halves were generally sub-divided into 16, 25-foot lots, each 140 feet deep (32 lots to the block maximum) (Graphic 2). The railroad station sat proudly in a park on the west side of Main Street and at the head of Fremont Street (the site of today's Union Plaza Hotel / Casino). The railroad's employees were the first occupants of the new town. Fremont Street was parceled up to be the main commercial street, running east-west against the grid. North-south streets ran parallel to Main and number First, Second, Third, and so on moving eastward. However, Fifth Street has since become Las Vegas Boulevard, extending "The Strip" northward into



MAP 3: DOWNTOWN: HISTORIC BLOCK AND PARCEL PATTERN

*Las Vegas was founded by the Union-Pacific Railroad on May 5, 1905, as a commercial real estate venture. The railroad's realtor laid out an original grid of 40 blocks, each 300 by 400 feet, long-wise along the train tracks, and at auction sold off lots on those blocks. A 20-foot wide alley broke most blocks in half, and the halves were generally sub-divided into 16, 25 foot lots, each 140 feet deep (32 lots to the block maximum).*

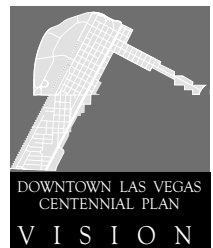




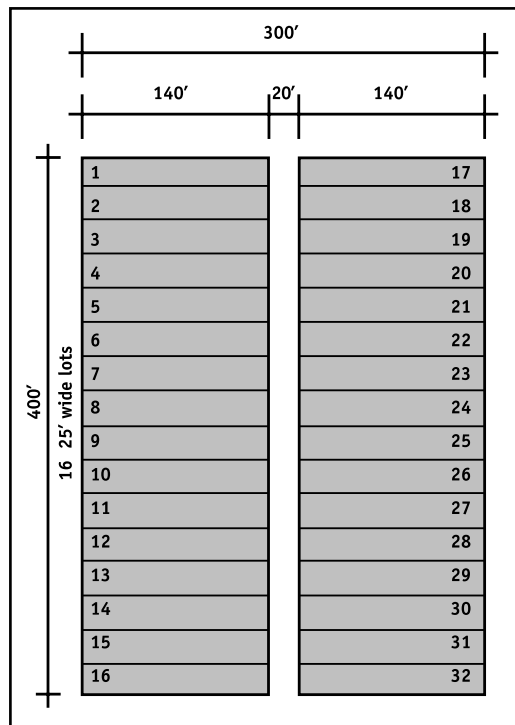
Downtown Las Vegas. East-west streets were named for famous folks who were influential in the founding of Nevada (Map 3). A mix of residential and commercial uses, including gambling halls and saloons, sprung up in this gridded, dusty desert. The City was incorporated under State of Nevada Statutes in 1911.

**2** Downtown Las Vegas has a rich legacy of business, commerce, entertainment, lodging, and community culture. It ranges from the founding of the First State Bank of Las Vegas on the corner of First and Fremont Streets in 1905 to the ultra, high-tech entertainment spectacular of today's Fremont Street Experience. Downtown Las Vegas has long served as the hub of the entire Las Vegas region (Map 4). Over the last generation though, office development has dispersed to new suburban centers like Summerlin, Green Valley, Hughes Center, and McCarran Airport Center. New housing development has spread out across the valley. Downtown Las Vegas, once the residential, financial, and cultural heart of the City, has been regularly passed over for the allure of new suburban oases.

**3** The original block pattern, and especially the small parcel layout, presents the greatest challenge today for downtown redevelopment (Graphic 2). Many 25 and 50 foot wide lots still remain in the Office Core District and across downtown, under



GRAPHIC 2: ORIGINAL DOWNTOWN BLOCK PATTERN AND SMALL PARCEL LAYOUT





separate ownership. Modern development seldom or never happens in 25 foot wide increments. The smallest parcel feasible for current development is larger than a half block, with many on a full block or larger. Also, the entire Office Core area and most of downtown were removed from the Gaming Enterprise Overlay District by City Council in 1996, and the Nevada State Legislative in 1997, to relieve the artificially high land values. This will work to concentrate casino activity along the Fremont Street Experience area.

PHOTO 11: FREMONT STREET IN 1911, AT TIME OF INCORPORATION

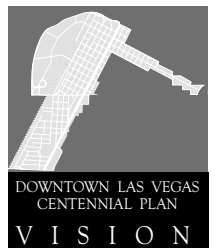


PHOTO 12: FREMONT STREET IN 1958

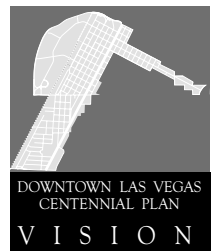


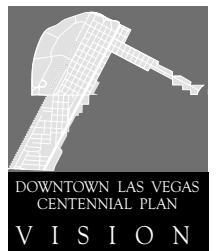
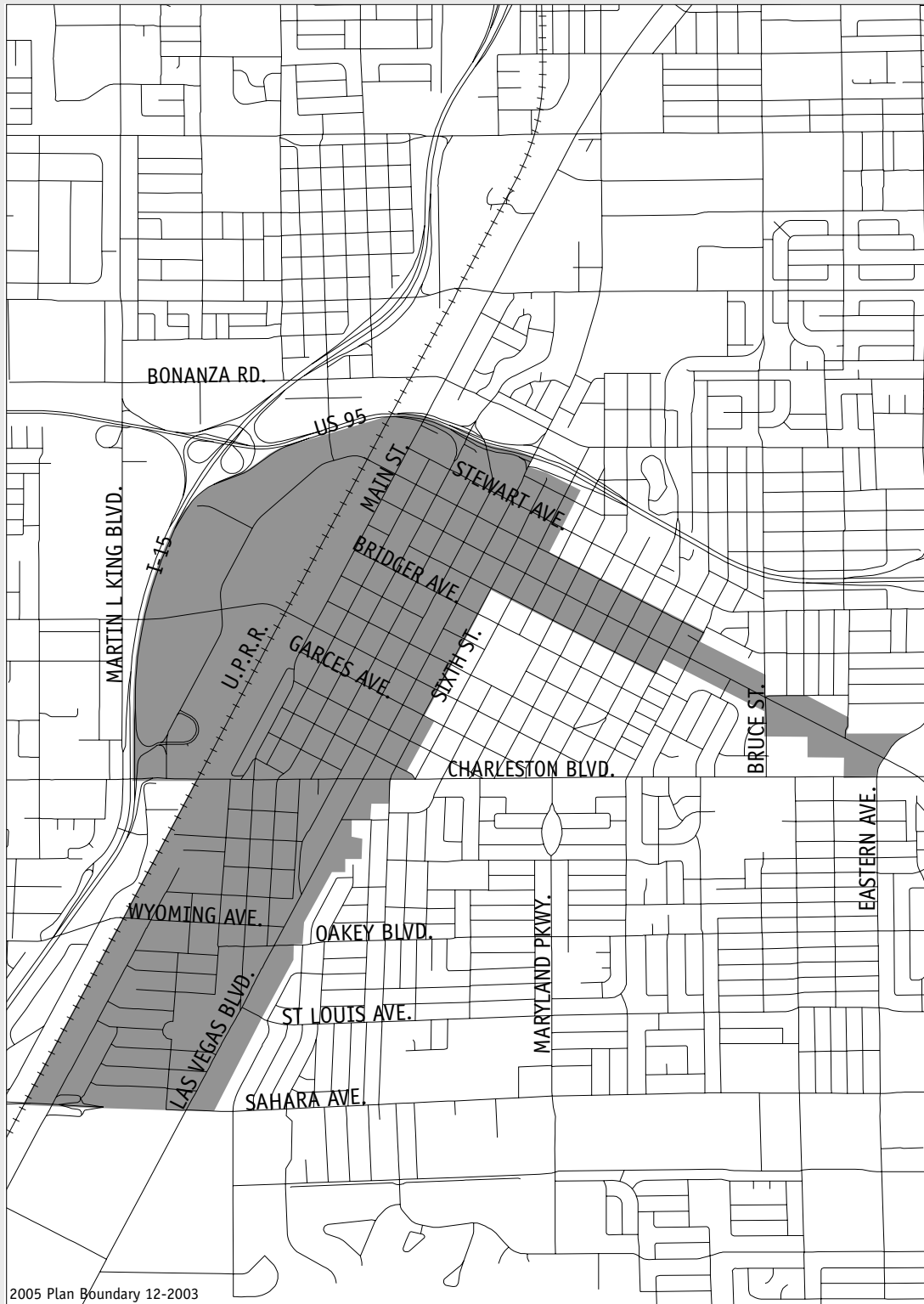
### V2A. AREA OF STUDY

**1** Within the context of this Plan, Downtown Las Vegas shall be defined to mean the geographic area generally contained within the boundaries of the northern parcels along Bonanza Road on the north, eastern parcels along Sixth Street on the east, Sahara Avenue on the south and Interstate-15 on the west, and extending along East Fremont Street to Charleston Boulevard. The centerline of Sahara Avenue is the boundary between the City of Las Vegas and unincorporated Clark County. This land area, approximately one-half mile wide and one and one-half miles long, is divided into seven unique districts. This area contains nearly 1600 acres (Map 5).

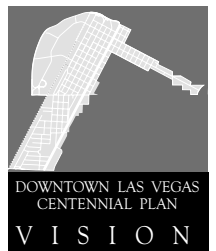
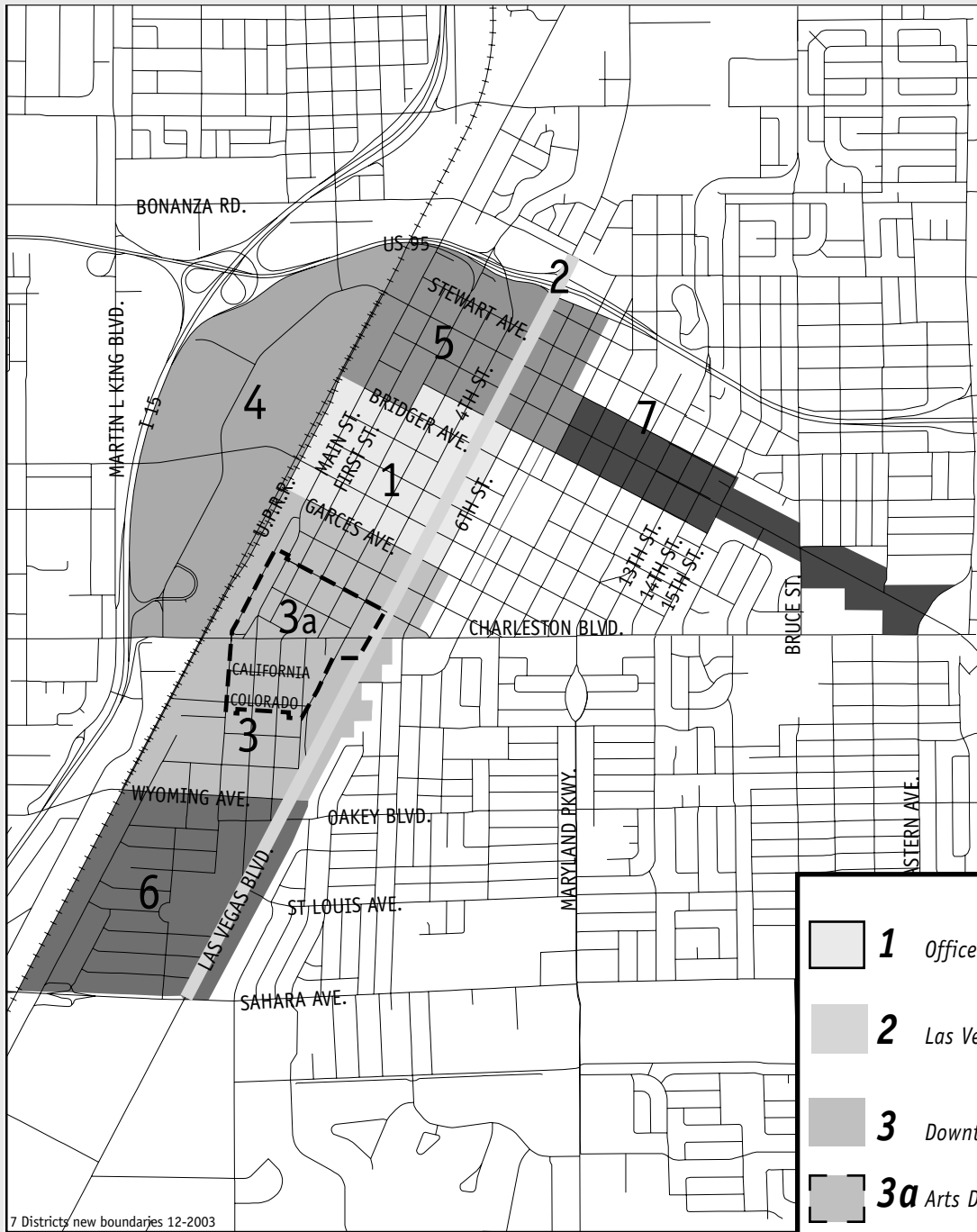
**2** This area of study is the major, central portion of the City of Las Vegas Downtown Redevelopment Area, but excludes the Redevelopment Agency's surrounding local neighborhood centers which are not a part of this plan. In June 1998, City Centre Development Corporation (CCDC), in agreement with the City Council sitting as the Agency, expanded its area of influence to include much of this area of study.

**3** The area of study is conceptually divided into seven planning districts (Map 6). The boundary and character of each district is defined by the predominant type and intensity of land uses, general architectural character, and qualities of the streetscape within that area. All the districts are linked together by Las Vegas Boulevard commonly called "The Strip" to the south or "The Boulevard" as it passes through the City and beyond its limits to the north.





MAP 5: LAS VEGAS DOWNTOWN CENTENNIAL PLAN BOUNDARY



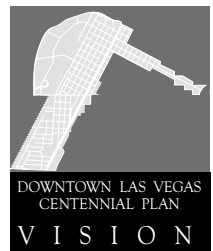
- 1** Office Core
- 2** Las Vegas Blvd.
- 3** Downtown South
- 3a** Arts District
- 4** Parkway Center
- 5** Casino Center
- 6** Northern Strip
- 7** East Fremont

MAP 6: SEVEN DOWNTOWN PLANNING DISTRICTS

1 Efforts to competitively reposition Downtown Las Vegas began in April 1986 when the City of Las Vegas Downtown Redevelopment Agency (DRA) engaged the consulting firm of Laventhol and Horwath, CPA, of Los Angeles, to prepare a development analysis and strategy for Downtown Las Vegas. The policies in that plan and strategy enabled the DRA to react to developer proposals as they were presented. This strategy predominated until 1993 when the DRA Board of Directors sought to establish a more proactive stance to the redevelopment efforts of Downtown Las Vegas. The firm of Trkla, Pettigrew, Allen & Payne, Inc. (TPAP) was engaged to update the agency's comprehensive plan and redevelopment strategy.

2 A summary of the primary observations of the 1993 TPAP study include:

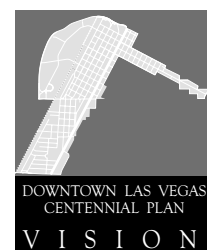
- a. Existing conditions do not contribute to significant redevelopment goals. Downtown Las Vegas is the original Office Core, but does not have office uses typically found in most Central Business Districts. Most tenants are government offices and related uses. Many old, single-family homes have been converted to offices that primarily house attorneys and accountants. "Prestige" addresses are not found downtown, but are located in remote, sub-market areas like the Hughes Center, Summerlin, Green Valley and office parks around McCarran Airport.
- b. Downtown Las Vegas redevelopment efforts will increase a positive "critical mass" of diverse but complimentary uses, halting the trend of "fractionalized" office activity.
- c. Seven planning areas were identified as sub-components of Downtown, each with its unique characteristics, opportunities, and constraints.
- d. Casino Center, the historic concentration of casinos lining Fremont Street and its adjacent streets, will conserve and enhance the pedestrian character with tourist-serving uses including hotels, casinos, specialty retail, restaurants, and entertainment attractions.
- e. Entryway design improvements into Downtown should be implemented at key gateways and should include special lighting, signage, landscaping, and other features.
- f. The Office Core will promote the return of Downtown Las Vegas as the region's legal, governmental, and financial center. Office uses north of Garces Avenue will reinforce the image of the major governmental, business, and financial center while the blocks south of Garces should be mixed-use office, residential, and commercial uses woven into the urban fabric. Ground floor retail will be encouraged on the blocks north of Bonneville. Low-intensity office uses with high-to-medium density residential will dominate the urban character south of Bonneville.



- g. The City should work with the Clark County Assessor to determine the appropriate and fair assessments of parcels and improvements within the downtown area, increasing assessments to at least 90 percent of current market value, or comparable. This will encourage divestiture of under utilized parcels and assist with assembly of small lots into useful redevelopment sites.
- h. Las Vegas Boulevard should become an attractive “identity street” as it passes through the Downtown Las Vegas area. It should consist of distinctive lighting, landscaping, and signage with its own urban design standards establishing the character and restrictions of development to occur within its edges. New development along the Boulevard should include commercial uses such as specialty retail, hotels, entertainment, and commercial services. The uses should be differentiated from similar types found in Casino Center and the Office Core, allowing them to retain their low-rise, smallscale character for commercial, governmental and institutional uses.

**3** Building upon the urban planning goals and framework provided in the 1993 TPAP study, the Denver office of the urban planning / design firm, EDAW, Inc. (in consultation with the market research firm, Growth Management, Inc., real estate development advisors, Keyser Marston Associates, Inc., and transportation consultant, TDA, Inc.) drafted an urban design master plan for the City of Las Vegas, City Manager’s Office. EDAW’s draft plan was adopted by Resolution of the City Council on October 2, 1996, as “draft design guidelines” only. The EDAW plan was intended to translate the TPAP urban planning and framework goals into a plan for future improvements, and to establish appropriate land use configurations in conjunction with design standards for image, character, landscaping, building massing, pedestrian systems, and public open space.

**4** The CCDC was formed by the City Council in late 1997 to oversee redevelopment activities in Downtown Las Vegas. City of Las Vegas Planning and Development Department staff, with the advice and consent of the CCDC Downtown Implementation Sub-Committee, edited, redrafted, and completed the Plan on February 2, 2000. The Plan was adopted by a resolution of the CCDC Board of Directors on February 17, 2000. CCDC recommended adoption of the plan to the Planning Commission and City Council, and it was adopted by Ordinance 2000 - 49 on July 5, 2000, as a General Plan Amendment.



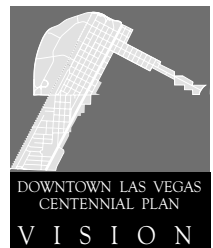
## V2C. APPLICABILITY OF THIS PLAN

19

**1** The Downtown Las Vegas Centennial Plan conforms to the General Plan of the City of Las Vegas, and is hereby incorporated into the City's 2020 Master Plan by way of Ordinance 2000-49, adopted July 5, 2000. The Downtown Las Vegas Centennial Plan supersedes and replaces the Downtown Urban Design Master Plan as adopted by Resolution as "Design Guidelines" by City Council on October 2, 1996.

**2** The Downtown Las Vegas Centennial Plan complements and coordinates with the new Downtown Overlay District, Title 19.06.060 of the City of Las Vegas Zoning Code, as adopted March 1997 and amended thereafter, and is incorporated therein by Ordinance 2000-49 adopted July 5, 2000.

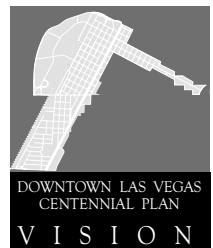
**3** The Downtown Centennial Plan complements and coordinates with the Redevelopment Plan for the Downtown Las Vegas Redevelopment Area, as adopted March 5, 1980, and amended thereafter.



**1** The Urban Design Framework establishes the following urban design concepts and general urban planning principles for Downtown Las Vegas.

### V3A. GATEWAYS

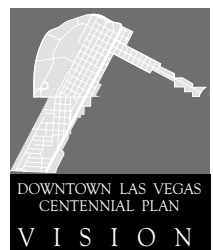
**1** Entry points to important places are largely ceremonial in nature, symbolically marking the point of passage from one district into another. Las Vegas has several entry points into the downtown area, many of which are not now distinctively delineated. The gateways into downtown should receive special treatment, including landscaping, lighting, signage, and other related improvements. This will make them distinctive within the urban environment and establish a civic presence and sense of pride for visitors, workers, and residents of downtown. Secondary gateways between the Office Core District and surrounding districts and neighborhoods should also feature physical improvements of landscaping, lighting, and signage to announce that transition.





1 Las Vegas is world renowned for its casinos and entertainment. This Plan builds upon that status in its delineation of substantial casino districts, perpetuating an image, character, and history that is uniquely Las Vegas. In downtown, the “historic” Casino Center District along Fremont Street to the north and the northern Strip casino district around the Stratosphere Tower to the south form two distinct areas of landmark status. The visual prominence of the Stratosphere Tower as the compass point for the Las Vegas Valley and the classic Las Vegas casinos nestled into the urban fabric along Fremont Street Experience provide downtown with two entertainment and activity anchors.

PHOTO 13: FREMONT STREET EXPERIENCE PLAZA - ENTERTAINMENT ANCHOR



### V3C. SPINE

**1** Las Vegas Boulevard is the downtown identity street. The Boulevard south of downtown is simply known as “The Strip.” Lined with casinos and the characteristic brilliant signage, it has established Las Vegas’ image as no other urban element. This Plan utilizes that imagery by extending the landscape continuity north into Downtown Las Vegas and building upon the legacy of The Strip, thereby forging a new identity for downtown. Las Vegas Boulevard forms the major north-south spine downtown. Through landscape improvements, right-of-way width expansion, lighting, signage, and urban design standards, it will become a key urban design and orientation element for downtown.

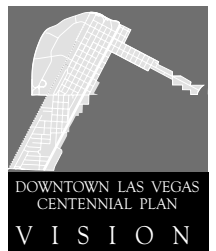
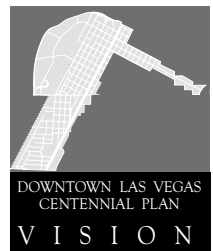


PHOTO 14: LAS VEGAS BLVD. LANDSCAPE IMPROVEMENTS, CREATING A “SPINE”



**1** Major vehicular and pedestrian linkages connect important places in the City, including anchors (like the casino districts), landmarks, neighborhoods, public uses, and private development. A system of linkages bolsters the integration of uses and overall design character. The City, with the Regional Transportation Commission (RTC), continues to widen and improve roadways not only entering and exiting downtown but throughout the entire Las Vegas valley.

**2** The RTC's proposed fixed guideway public transit system is moving forward with partial federal funding, with the downtown phase of construction to be up and running by 2005. Recent discussions suggest that the first phase of the Transrapid High-Speed Train, which will ultimately connect Downtown Las Vegas to Los Angeles and Anaheim, California, could well move ahead in the next decade with federal support for at least the first phase connecting Las Vegas to Primm, Nevada (at Stateline). The development of both systems, in addition to the potential restoration of Amtrak service, the continuation of Greyhound bus service, and the necessary relocation of the Downtown Transportation Center for the CAT bus system to a larger location and link it to multiple rail lines, strongly suggest the future need and opportunity to develop a "Downtown Transportation Hub", or Intermodal Transportation Center. Such a center could serve as an obvious linkage between downtown and the old Union Pacific railyards site (called Parkway Center within the context of this Plan), and include substantial parking, retail and commercial facilities.



**3** Paralleling Las Vegas Boulevard, Fourth Street serves as a primary north-south linkage, enabling one-way vehicular traffic northbound with accompanying special treatments for the pedestrian realm, including unique paving, landscaping, and lighting elements. It serves as an alternative entryway and thoroughfare for downtown, relieving traffic congestion on Las Vegas Boulevard and connecting directly to interstate entry ramps. Careful consideration should be given to completing this oneway couplet by rebuilding Casino Center into a southbound one-way route from approximately Clark Street southward.

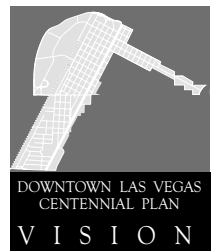
**4** Lewis Avenue should be developed into the primary east-west pedestrian corridor for the revitalized Office Core District. The intense concentration of new public and private developments along this street warrants the City's commitment to rebuild this street into the Lewis Avenue Corridor, or as some are already calling it, the new Civic Corridor because it links the Clark County Regional Justice Center and expanded jail

facilities with the new Federal Building / U.S. Courthouse. The Intown Office Building and the possible public-private re-use and revitalization of the historic Fifth Street School complex present enormous opportunities to again center business downtown. This critical mass of buildings, office workers, and visitors to the area will facilitate the redevelopment of surrounding blocks for years to come.

**5** Third Street, from Charleston Boulevard to Fremont Street Experience, should be developed as a major pedestrian linkage to unite the Arts District, the Office Core District and Casino Center. Widened sidewalks, directional signage and a greatly enhanced pedestrian experience will contribute to the revitalization of this central downtown area. The pending future vacation of the old Clark County Courthouse and other vacant public holdings present a great opportunity to redevelop several half or full blocks along the Third Street Pedestrian Connector. In the future, consideration should be given to carrying right-of-way improvements south of Charleston as commercial and new residential development warrants in the Downtown South District.

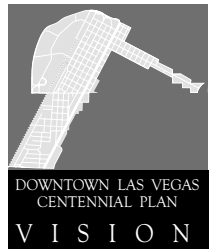
**6** Fremont Street Experience, running between Main Street and Las Vegas Boulevard, serves as a major east-west pedestrian connection, linking most major downtown casinos together and providing a pleasant, themed entertainment and strolling environment. It is a major local landmark and downtown destination center with its high-tech canopy providing entertainment by night and a shaded, breezy streetscape for daytime strolling. The development of Neonopolis, an urban destination entertainment center, coupled with the retail / family entertainment tenanting of the Fremont Street Garage frontage, will anchor the eastern end of Fremont at Las Vegas Boulevard and certainly encourage further redevelopment of the surrounding area. Strong consideration should be given to creating a pedestrian and visual linkage to the Union Pacific site as it develops to the west of Fremont Street Experience and the Office Core District. In addition, a direct pedestrian link from the future Las Vegas Monorail station should be developed in order to encourage alternative means of travel to Fremont Street and reduce vehicular congestion in the area. Fremont Street Experience could also be extended eastward for one or two blocks, which would assist in drawing pedestrian traffic to the emerging Entertainment District.

**7** Charleston Boulevard and Bonneville Avenue (or the proposed Bonneville / Clark paired one-way couplet) will serve as primary east-west downtown connectors, linking downtown to interstates and surrounding neighborhoods. Since Alta Drive is soon to be widened to the west of the Union Pacific property, a gateway feature should be built as Bonneville Avenue enters downtown at Main Street. Charleston should also feature a gateway entrance near its intersection with Main Street and include special landscaping, lighting, and signage to make it distinctive.

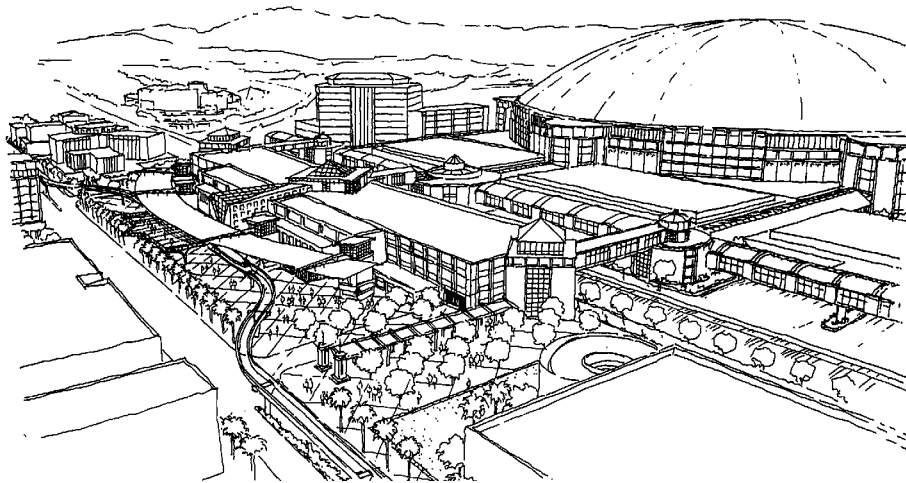


8 The most pivotal linkage in downtown will occur on Main Street. It will anchor the alignment of a fixed guideway (monorail) transit system, linking downtown to The Strip to the south and potentially McCarran International Airport. Stations scheduled along Main Street will benefit the Fremont Street Experience, Office Core District, development of the Union Pacific site, Downtown South District, and, eventually, the Northern Strip area. Main Street will continue to serve as an alternative route for local north-south downtown traffic. The possible future connection of Main Street with Industrial Road to the south of downtown could provide a continuous service axis to The Strip's western backdoor.

9 The net effect of all linkage systems and improvements will result in drawing people from one place to another, improving the image and character of the area, and consequently encouraging a greater use of Downtown Las Vegas in its entirety.

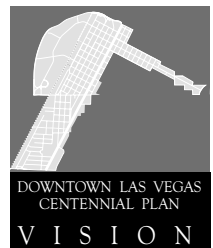


GRAPHIC 3: A HYPOTHETICAL MONORAIL STATION LINKING INTO A STADIUM OR ARENA



**1** Creating a broad diversity of land uses in Downtown Las Vegas is key to its future success as an activity center and vital urban regional hub. It should have a hearty balance of cultural, residential, commercial, office, civic, light industrial, and special uses.

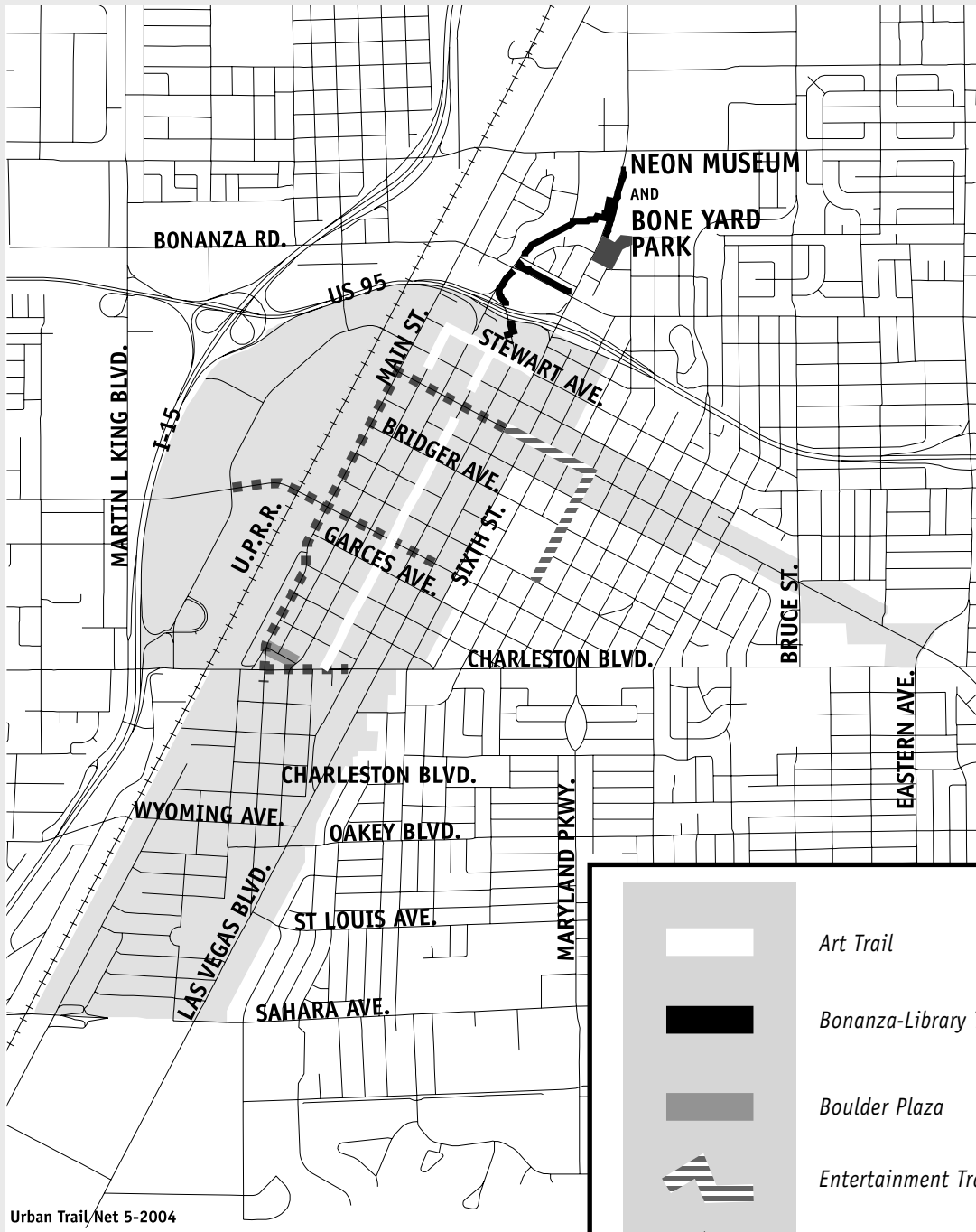
**2** Residential Neighborhoods: The success of downtowns across the country often depends upon integrating a successful residential community in, among, and adjacent to the downtown area. Las Vegas has a great opportunity to integrate a stronger residential community into the future of Downtown Las Vegas by building upon the existing infrastructure already in place. The City has identified the potential of building some 5000 residential units over the next 10 years within downtown Las Vegas. The creation and strengthening of neighborhoods is essential. It needs more than just the development of housing types that may focus inwardly without integrating to the physical and social structure of the surrounding community. Creating a neighborhood would include providing improved police protection, parks, schools, community centers, shopping, and other similar uses. The successful integration of residential development into Downtown Las Vegas will require focusing on efforts in specific areas, establishing a critical mass, and then transitioning to other areas one zone at a time. Individual successes will build upon the previous effort, and the whole of the community will be built over time, utilizing the momentum gained from the earlier successes.



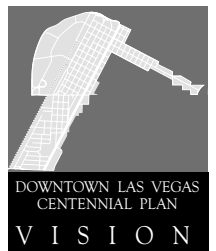
**3** Parks and Open Space: Many downtowns across the nation are recognized for their formal public open space, such as William Penn's four urban greens breaking the grid of Philadelphia, Washington Square in Manhattan's Greenwich Village, and Boston's Common. Over the next few years, the City should pursue the creation of a series of small urban park spaces linked by trails and enhanced pedestrian streetscapes to expand the livability of Downtown.

a. A central public open space or park should be developed in each of the Downtown districts. Parks and open space provide a focus for the neighborhood, offer relief from the urban environment, serve as a gathering space for neighborhood events, and offer outdoor recreation space for office workers and residents. The types of public open spaces appropriate for the existing urban fabric include plazas, squares, greens, formal gardens and small parks.

b. Several near future opportunities are looming for the development of parks and public spaces. The pending federal disposition of the 1933 United States "Old" Post Office Building at Stewart and Third and the adjacent development of Frank Wright Park will present the City with the opportunity to reprogram it for civic and cultural



Urban Trail/Net 5-2004



MAP 7: URBAN TRAIL NETWORK

uses, and redevelop the surrounding area as green space. The provision of an urban trail from the Post Office to the Cultural Corridor north of Downtown will further increase the viability of the structure for cultural programming. Undeveloped blocks on the south side of Fremont Street at Maryland Parkway afford the chance to devise a full block urban park and introduce new residential uses to center the east Fremont district. In the arts district, the Boulder Avenue right-of-way has been identified as a potential site for an urban plaza and sculpture garden. The redevelopment potential of the Downtown South District suggests the strong desire to create an entire urban block as a formal park, and surround it with new residential and mixed-use construction.

c. Non-vehicular urban trails should be established to connect all public open spaces, major anchors and transportation nodes. An urban trail, which is defined as a designated pedestrian route located in an urban area, will consist of widened sidewalks in existing right-of-way corridors. The trails should be readily identifiable through sidewalk treatments, directional signage and banners, shade trees, street furniture, public art and other techniques. The development characteristics of each trail need not be uniform, and will probably vary somewhat based on existing conditions and the availability of public right-of-way. Every effort should be made to improve the safety of pedestrians, including the provision of specialized crosswalk signals, protected pedestrian crossings, and the use of distinctive paving materials for crosswalks. Traffic calming measures should be investigated as part of the development of urban trails.

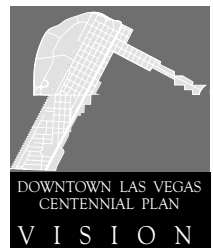
d. Urban trails to be developed downtown and should include the following areas and districts: (Map 7)

*Third Street:* Third Street should be rebuilt from Fremont Street Experience to Charleston Boulevard to have spacious sidewalks, shade trees and decorative plantings, thus serving as the primary north-south route linking the Old Post Office, Fremont Street Experience, Lewis Avenue Corridor, and the Arts District. The trail should then be extended south of Charleston Boulevard and provide a connection to The Gateway Landscape Feature.

*Monorail / Las Vegas Boulevard.* The extension of the Las Vegas monorail will assist in bringing additional residents and tourists to Downtown Las Vegas and reducing traffic congestion. East-west urban trails should be developed from each monorail station to Las Vegas Boulevard, as well as to major anchors in the downtown area.

*Entertainment District.* Extending an urban trail route from the east end of Fremont Street Experience down Fremont Street will assist in drawing visitors to the entertainment district and assist in the revitalization of that area. Providing an extension of the trail down Ninth Street to the Las Vegas Academy of Performing Arts Theater will further connect arts and entertainment venues in the downtown area.

*Parkway Center.* The development of a jogging and exercise trail as a green buffer between Parkway Center and I-15 should also be considered as part of the development of the urban trail network.





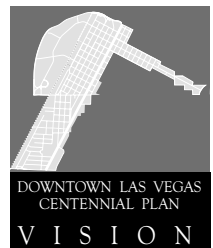
*Cultural Corridor.* Although outside the boundaries of the Downtown Centennial Plan, efforts should be made to develop a trail connection that will link the institutions of the Cultural Corridor with the Old Post Office, Frank Wright Park and other Downtown attractions. Establishing a pedestrian connection between these two areas will increase tourism and give visitors to the Downtown area greater access to a variety of recreational and cultural attractions.

**4** Hotels & Casinos: Casino use is of particular importance to the history and character of Las Vegas. Downtown must ensure its role in the casino enterprise that is so much a part of local culture and national folklore. The downtown casino areas not only preserve the history and culture that has already been established, but ensure vitality and synergy among existing and future casinos. The casino areas delineated in this Plan illustrate areas that build upon and preserve those historical patterns while providing the opportunity for future expansion.

**5** Government Offices: Downtown Las Vegas is host to the City Hall complex and will soon house the new Regional Justice Center, Federal Courthouse, and Detention Center expansion. Other City and County offices are currently dispersed across downtown. These significant new projects, in conjunction with the expansion of the City Hall complex, require that the City establish appropriate strategies for accommodating a significant civic core downtown. The complex of structures should display a high quality architectural prominence and surround a significant civic plaza that would serve as a focal point for public activity and events.

**6** Commercial Office Core: The primary Office Core for Downtown Las Vegas extends from approximately Garces Avenue on the south to Carson Street on the north and is bounded by the Union Pacific tracks on the west and Las Vegas Boulevard on the east. This area illustrates an overall district consisting primarily of office uses with the majority of large-scale, class A office space to be accommodated in its northern half. The northern portion of the overall district, or Office Core, can best be described as the central business district. It will be home to governmental and corporate office uses and support retail, restaurant, and other service uses necessary to complete an active Office Core. The southern half of the office district will be smaller scale office uses with predominantly single users in renovated structures or small office buildings. Other uses may be found in conjunction with this type of office use including residential, commercial, and support services.

**7** Light Industrial / Service Commercial: This area will be comprised of a mix of industrial and commercial services such as automotive service, paint shops, light manufacturing, laundry / cleaners, copy center, and similar types of uses. Its relationship to the railroad tracks and freeway system will continue to support light industry as well

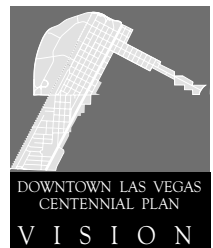


as facilitate commercial development in transitional areas nearer Las Vegas Boulevard and other arterial streets. Design standards should be established for the industrial area, particularly as it abuts the residential and casino areas, and along the fixed guideway system alignment. Its visual impact and the potential for disruptive features such as noise from trucks and trains, chemical usage, and other factors will create potential harmful and detrimental effects for users of adjacent properties.

**8** Commercial Corridors: The primary Downtown Las Vegas commercial district encompasses the Charleston and Las Vegas Boulevard corridors. It will be comprised of traditional Las Vegas commercial uses such as hotels and motels, wedding chapels, photographic studios, service commercial including convenience retail, specialty retail and other similar uses.

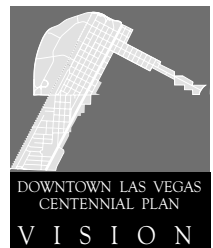
**9** Parkway Center: The former Union Pacific rail yards site to the west of the Office Core District have been rezoned in 1998 to Planned Development (PD), from its former Industrial (M) zone. The City anticipates a variety of land uses developing on the site, including non-gaming hotels; convention and arena facilities; and retail, residential, cultural, and mixed uses. Clark County will also continue to expand its government services campus. Pedestrian linkages to Fremont Street Experience and the Office Core will be very important to the City: Parkway Center must function as a natural extension of the existing downtown to enhance the businesses that already exist there. A tremendous opportunity is now emerging to develop an intermodal transportation center that would link together the fixed guideway, the Amtrak, Transrapid high speed train and, the CAT bus system with other existing forms of transportation. Such a station could also serve as a bridge between Fremont Street Experience or the Office Core and Parkway Center. It is recommended that this site be planned as a whole and not sold or developed on a piecemeal basis.

**10** Transit Oriented Overlay Zoning Districts: These overlay districts will be established around the location of each of the Monorail stations. The districts will permit and encourage the development of transit oriented live-work use centers that will provide economic stability and diversification for the areas adjacent to and in walking distance of fixed Guideway stations in the downtown area. The primary objective of the Transit Oriented Overlay Districts is to provide employment opportunities for individuals in and around a Guideway station (areas that will hereafter be called Transit Districts) while at the same time creating a unique blending of human habitation and economic activity.



**1** For the purposes of this Plan, Downtown Las Vegas has been divided into seven distinct and recognizable districts (Map 6). Each district has a particular variety of land uses, density, and urban character demonstrated by the existing building fabric it contains and the redevelopment opportunities it presents. All seven districts are small enough to be walkable enclaves unto themselves, yet the continuity of general design standards and streetscape design will weave them together into a continuous downtown urban experience. Downtown Las Vegas excludes the Las Vegas High School Historic District.

**2** District One: Office Core - The Office Core District, which is the primary concentration of professional and governmental offices and supporting pedestrian-oriented uses, is bound by the eastern edge of the Union Pacific Railroad right-of-way on the west, the centerline of Bridger Avenue between the tracks and Casino Center Drive, but then the centerline of Carson Avenue between Casino Center Drive and the properties on the eastern side of Sixth Street on the north, the centerline of Sixth Street on the east, and the centerline of Garces Avenue on the south (Map 6).



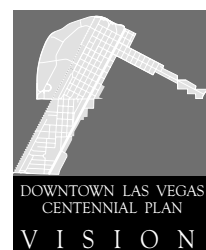
**3** District Two: Las Vegas Boulevard - The Las Vegas Boulevard District overlays all the other districts and is the primary tourist commercial and entertainment-directed spine that links The Strip to Downtown Las Vegas and its varied districts. The district is generally defined to include the Las Vegas Boulevard right-of-way and all the properties adjacent thereto on both the west and east sides of the right-of-way (Map 6).

**4** District Three: Downtown South - The Downtown South District is a mixed-use area containing a wide variety of land uses and density of existing utilitarian buildings and multifamily structures ripe for rehabilitation. The district is bound by the eastern edge of the Union-Pacific Railroad right-of-way on the west, the centerline of Garces Avenue on the north, the properties on the eastern side of Sixth Street to Charleston Boulevard but then the eastern side of Las Vegas Boulevard south of Charleston Boulevard on the east, and the centerline of Wyoming Avenue on the south, excluding the properties within the boundaries of the Arts District. The Downtown South District includes the area some are now calling the Gateway Park Neighborhood to the north and west of the Gateway Park entry feature, at the split of Fourth Street and Las Vegas Boulevard. (Map 6)

**5** District Three-A: Arts District - The Arts District contains a distinct mixture of arts-related businesses, single- and multifamily residences, and service commercial uses. It will continue to redevelop and contain an eclectic and urbane mix of businesses and residences including artist studios; cafes and coffee houses; art galleries; artisan and craftsperson storefronts and workshops; lofts and live/work situations; and diverse mixed-use developments within existing buildings and new developments. The district is bound by Commerce Street on the west, the properties on the south side of Colorado Street to the south, Fourth Street south of Charleston and Las Vegas Boulevard north of Charleston to the east, and Hoover Avenue to the north (Map 6). The block bounded by Fourth Street on the west, Hoover Avenue on the north, Las Vegas Boulevard to the east, and Charleston Boulevard to the south is included within the defined boundaries of the Arts District, but is not subject to Arts District design standards or uses. This one-block area shall be governed by the general development standards of the Downtown Centennial Plan and any other overlay standards that may be developed for Las Vegas Boulevard.

**6** District Four: Parkway Center - Parkway Center encompasses the entire former Union Pacific railyards, which in part is home to the Clark County Government Center. The remainder of the site is vacant and prepared for redevelopment opportunities, such as a performing arts center, a professional sports stadium, offices, high density residential, and commercial uses. Parkway Center should become a dense, walkable, mixed-use environment linked to the existing downtown street grid. The district is bound by the eastern edge of the Interstate 15 right-of-way on the west, the southeastern edge of the “Spaghetti Bowl” and U.S. 95 / Interstate 515 on the north, the western edge of the Union-Pacific right-of-way on the east, and the centerline of Charleston Boulevard on the south (Map 6).

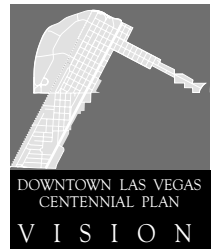
**7** District Five: Casino Center - The Casino Center District is home to the historic and classic Las Vegas casinos of Glitter Gulch, the fabulous Fremont Street Experience light and sound extravaganza, and the new and exciting Neonopolis. The District is also home to Las Vegas City Hall, the Downtown Transportation Center, the Old Post Office, and other related land uses. Casino Center is bound by the eastern edge of the Union-Pacific right-of-way on the west, U.S. 95 / Interstate 515 on the north, the properties on the eastern side of Seventh Street between Interstate 515 and Ogden Avenue, but then the centerline of Las Vegas Boulevard between Ogden Avenue and Carson Avenue on the east, the centerline of Bridger Avenue between the tracks and Casino Center Drive but then the centerline of Carson Avenue between Casino Center Drive and Las Vegas Boulevard on the south (Map 6). (The remaining areas north of Interstate 515 that are within the CCDC Area of Influence but not now included within this district could be



added to this district in the future or combined with additional parcels to the north to form a future Downtown North District.)

**8** District Six: Northern Strip - The Northern Strip District centers around the Stratosphere Tower Hotel and Casino and includes the old Meadows Village Neighborhood to its west. This transitional district is the ideal location for future major hotel-casino projects adjacent to the Stratosphere and also along Sahara to seamlessly extend The Strip northward into Downtown Las Vegas. Future evaluations will have to determine the highest and best land uses for this area. The district boundaries are the eastern edge of the Union-Pacific right-of-way on the west, the centerline of Wyoming Avenue on the north, the properties on the eastern side of Las Vegas Boulevard on the east and the centerline of Sahara Avenue on the south (Map 6).

**9** District Seven: East Fremont - The East Fremont District is the logical extension of the Fremont Street Experience eastward and includes the El Cortez Hotel and Casino, other small casinos, and supporting commercial, motel, and residential uses. This transitional district is bound by the centerline of Las Vegas Boulevard on the west, the centerline of Ogden Avenue on the north, the centerline crossing of Eastern Avenue and Charleston Boulevard on the east and the centerline of Carson Avenue on the south. The East Fremont District will present future opportunities for new commercial ventures and the integration of medium density mixed-income residential neighborhoods. The potential for open space to center these new neighborhoods should be investigated in the area of Fremont Street and Maryland Parkway (Map 6).

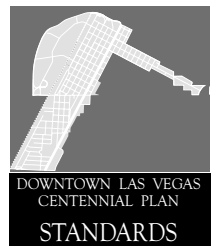


## DS. DOWNTOWN LAS VEGAS: DESIGN STANDARDS

### DS1. URBAN DESIGN GOALS AND OBJECTIVES:

- 1** These are the design goals and objectives for land use in Downtown Las Vegas:
- a. Create retail and other “pedestrian-friendly” uses on the ground floor of buildings, particularly in identified districts and corridors.
  - b. Create a better definition of the districts in and around downtown for more consistent, managed design character and image related to the uses therein, and to achieve a compatibility of uses between and among districts.
  - c. Encourage new housing in downtown where appropriate.
  - d. Encourage diversity of land uses within a guiding framework to achieve long-term vitality and economic growth.

- 2** These are the design goals and objectives for urban form in Downtown Las Vegas:
- a. Bring building facades to the property line / sidewalk edge along primary pedestrian streets.
  - b. Require landscaped setbacks along primary vehicular streets.
  - c. Require innovative treatments related to parking structures, incorporating retail or commercial uses into the ground floor and restricting facade treatments to appropriate architectural finishes.
  - d. Develop districts with consistent and cohesive architectural massing, establishing development standards for Floor Area Ratio (FAR), building heights, and streetwall formation.
  - e. Capitalize on recent development momentum created by the opening of the Fremont Street Experience, the proposed Regional Justice Center, Federal Courts Building, Neonopolis, and the potential expanded City Hall complex.



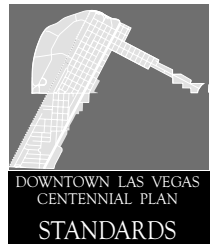
**3** These are the design goals and objectives to create a pedestrian environment in Downtown Las Vegas:

- a. Create streetscapes that provide safety, comfort, and interest for pedestrians.
- b. Establish a network of urban open spaces consisting of corridors, nodes, and link-ages.
- c. Encourage the provision of arcades, particularly in the Office Core.

**4** These are the design goals and objectives to enhance image and character in Downtown Las Vegas:

- a. Enhance the overall image of downtown through improvements at gateways, primary corridors, civic plazas, and open spaces.
- b. Develop an alley beautification and management plan.
- c. Establish a sense of place based on regional culture, climate, and environmental elements unique to Downtown Las Vegas.

**5** The following Design Standards apply throughout all of Downtown Las Vegas and are applicable to all seven districts.



**6** The purpose and intent of establishing Downtown Las Vegas design standards is to restore vitality and economic opportunities, give overall direction to future public and private improvements throughout the area. When applied, they will create a downtown that will have a unique, identifiable, high quality, vibrant image and character. The standards will enrich the pedestrian experience downtown through improved streetscapes, landscaping, parks, plazas, and open space. These design standards, in conjunction with the land planning strategies, will redefine Downtown Las Vegas as an attractive, safe, economically vital regional center for business, commerce, government, entertainment, and the arts.

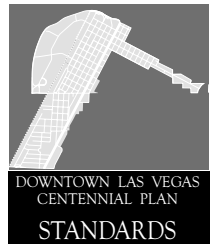
**7** Where different standards or elements are included within the District Plans contained herein, such as the Office Core District Plan, they shall supersede the Downtown Las Vegas and other existing standards.

**8** Waivers to the Design Standards contained within the Downtown Las Vegas 2005 Plan may only be granted by the City Council with clear and convincing evidence for demonstrated economic hardship or to further the City's redevelopment efforts.

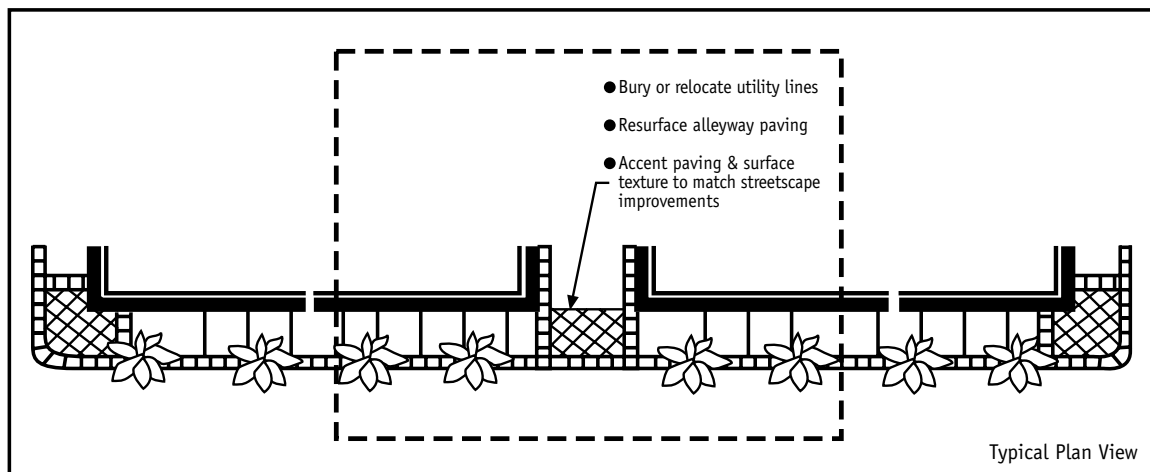
## DS2. DOWNTOWN LAS VEGAS SITE PLANNING STANDARDS

36

- 1** These are the site planning standards for all of Downtown Las Vegas:
- a. All development proposals for new buildings and structures shall conform to the minimum parcel size, if any, that is specified in LVMC Chapter 19.08.
  - b. All other development standards for new buildings and structures shall be as set forth in LVMC 19.06.060.
  - c. Plan all structures to conceal service areas and storage areas from public street view. Incorporate these uses into the main building whenever possible.
  - d. All auto-related facilities (working bays, storage, etc.) shall orient away and be screened from public view. When parking lots face major public streets, ornamental screen and landscaping shall be incorporated, as per Graphic 5.
  - e. Buildings shall orient their public entrances toward major street frontages as much as possible, particularly on major pedestrian streets.
  - f. Utilities and power service lines shall be located underground for all new development. Any new utility or power service line provided to a parcel shall be placed underground from the property line to the point of on-site connection or service panel location. To the extent possible, relocation of utilities from alleys (above ground) to streets (below ground) shall be undertaken with all new development activities, adjacent to their developments.
  - g. When alleyways are abandoned and building envelopes expanded to create larger development parcels and to eliminate negative character of existing alleys, the developer shall relocate utilities and design sidewalks to approved design. If the alley is not abandoned, it shall conform to the standard Alleyway Treatment (Graphic 4). The owners of parcels which abut alleys shall be required to provide for their proportionate share of the Alleyway Treatment pursuant to a schedule as adopted by the City Council.



GRAPHIC 4: STANDARD REQUIRED ALLEYWAY TREATMENT, IF ALLEY NOT VACATED.





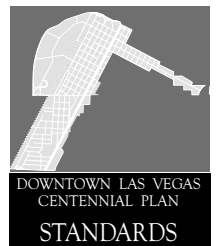
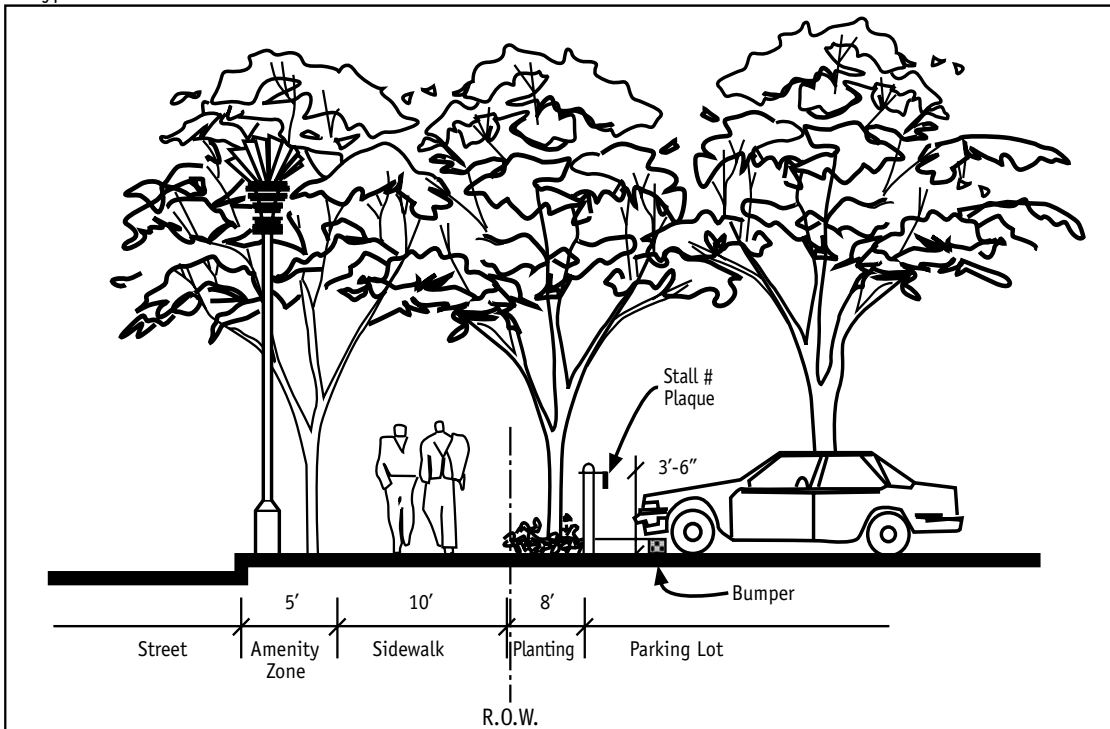
## DS3. DOWNTOWN LAS VEGAS TRANSPORTATION AND PARKING STANDARDS

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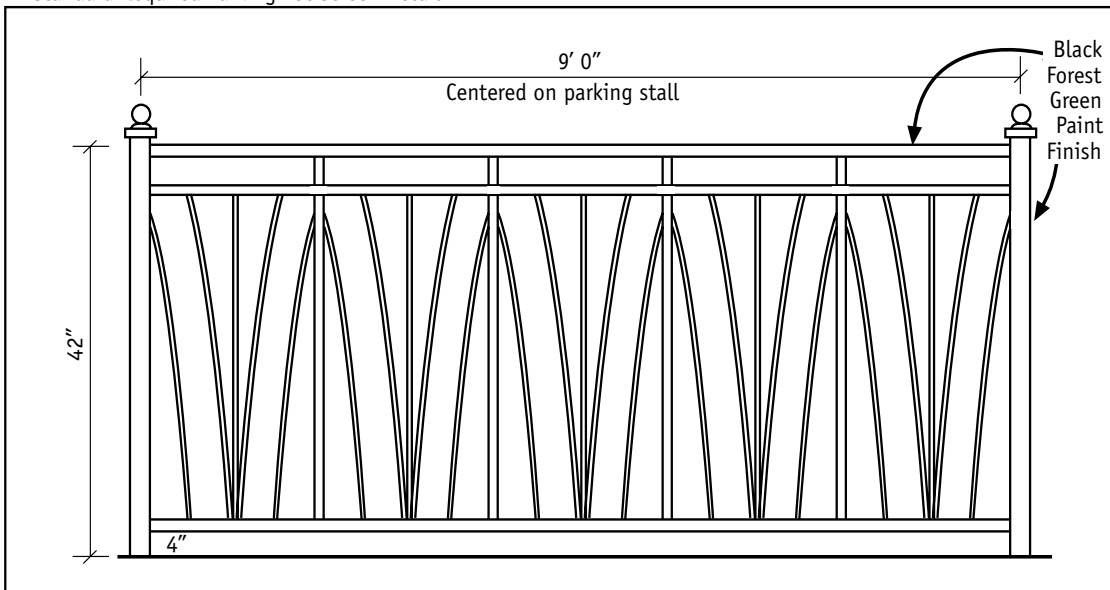
- 1** These are the circulation and parking standards for all of Downtown Las Vegas:
- a. Transportation systems shall be integrated to provide efficient and effective access, parking, and multi-modal opportunities to reduce congestion and pollution.
  - b. Any on-street parking shall not count in meeting parking spaces as required. Limited, on-street parking may be provided as a reserve of short-term parking or for service and delivery purposes.
  - c. All off-street surface parking lots shall be paved and include access aisles and drive-ways. Surface treatments including paving and striping shall be maintained in good condition at all times.
  - d. Whenever a driveway is abandoned, the owner shall remove all driveway pavement and approach, re-install curb and gutter, and landscape the area to match the adjacent landscaping / streetscape.
  - e. Parking areas shall be separated from buildings through surface treatment variations and / or grade separation. Parking spaces that directly abut buildings shall be prohibited.
  - f. Wherever possible, adjacent parcels each requiring access drives shall share a common, centrally located access drive.
  - g. Parking lots facing public streets, major pedestrian walkways, or plazas and open space shall incorporate a minimum eight foot wide landscaped setback with parking lot screen along the entire length of visual exposure (Graphic 5).
  - h. Surface parking lots shall have a minimum of one shade tree for every six spaces in landscaped islands and / or on the perimeter of the lot. A minimum of ten square feet of landscaped surface area must be provided for each parking space, incorporated into islands and / or perimeter landscape treatment, as per Graphic 5.
  - i. Bumpers or tire stops shall be provided in all parking stalls heading into a sidewalk, planting area, street, or alley. They shall be installed at a minimum of two feet from the beginning of the sidewalk, planting area, street, or alley in order to effectively protect walkways, landscaping, and signage (Graphic 5).
  - j. Above grade parking structures shall be set back from streets a minimum of ten feet, except where desirable ground floor uses such as restaurants, retail, and commercial facilities are incorporated into the first floor of the structure facing the street. In this case, the setback is not required and appropriate build-to line designation zones shall apply. The ten foot required setback shall have sufficient landscaping to screen the structure at the pedestrian level, thus providing a visual amenity (Graphic 9).
  - k. All new developments shall provide and install standard Fourth Street style fixtures in place of existing fixtures. Exact specifications, shop drawings, and standard suppliers can be obtained from the City of Las Vegas Engineering Design Superintendent, Department of Public Works, 229-6272.
  - l. Exterior walls of all parking structures shall be designed as part of the architectural form of the main building or surrounding buildings. (The intent is to minimize a garage



Typical Street Section



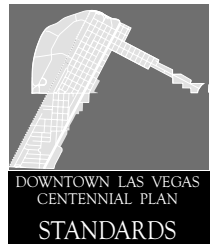
Standard Required Parking Lot Screen Detail



appearance through designs that conform to the accompanying development's general architectural features.)

- m. Bus drop-off areas shall be provided in close proximity to the entrances of newly constructed hotels. Parking for buses shall be accommodated off-site or in a remote, non-obtrusive location so as not to interfere with normal traffic flow or pedestrian activity to and from the hotel site.
- n. As appropriate with new development, bus turn-outs should be acquired by the City to accommodate bus stops outside the existing travel lanes.
- o. Parking, loading and traffic development standards other than those described above shall be as set forth in LVMC 19.06.060.

GRAPHIC 6: PARKING STRUCTURE WITH ARCADE AND CAFES



## DS4. DOWNTOWN LAS VEGAS STREETSCAPE DESIGN STANDARDS

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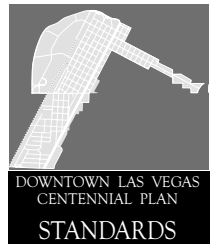
**1** Streets are the primary paths along which people move. They serve as the dominant organizing feature around which buildings, infrastructure improvements, and open spaces are organized. They should never be characterized simply as traffic channels. The idea of a street must extend beyond the right-of-way, property line, or building edge. Streets are inherently organized on a hierarchical system of movement and interest, and their role must be identified in the urban context in terms of image, character, and in functional requirements of linking uses.

**2** These are the streetscape design standards for all of Downtown Las Vegas:

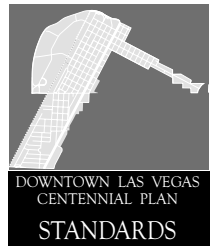
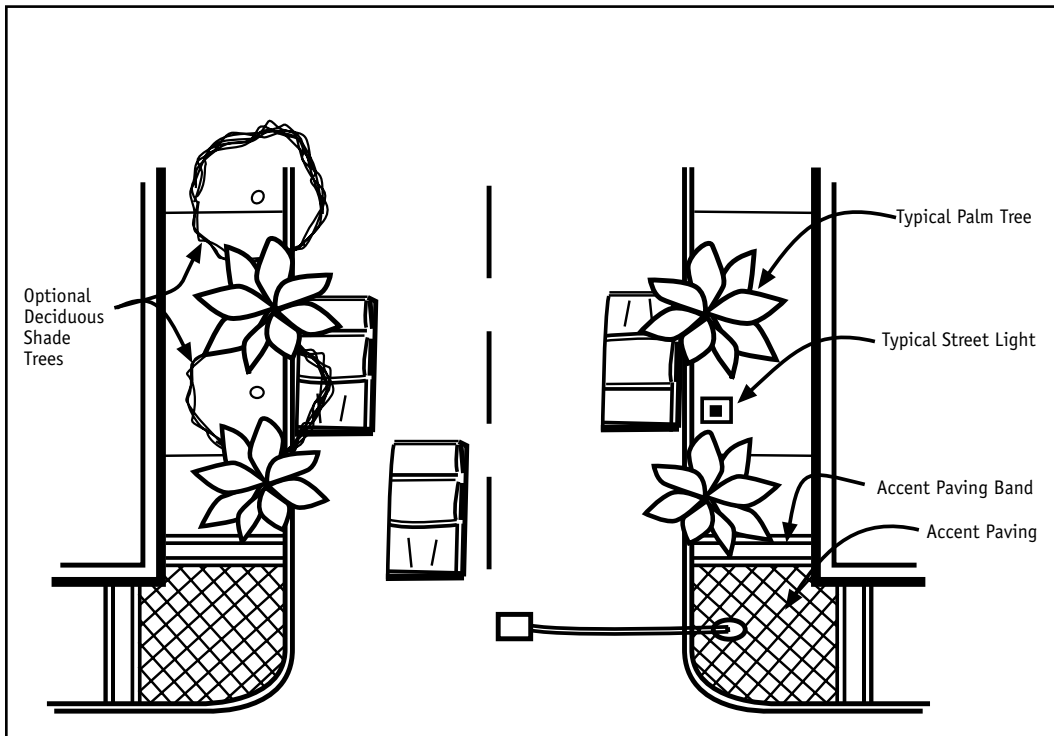
- a. North-south streets shall be designed thematically with palm trees as the primary landscape element (25 feet or greater in height with 30 foot spacing preferred and a maximum of 35 foot spacing). Shade trees, in single or double rows, may be provided alternately between palm trees to provide shade and pedestrian comfort (Graphic 7).
- b. East-west streets shall be designed thematically with shade trees as the primary landscape element (36 inch box trees spaced at 15 to 20 feet preferred to provide an immediate shade canopy, 24 inch box trees at 30 feet maximum spacing required). When shade trees are provided in lieu of a required arcade or other architectural shade device, a double row of 36 inch box (48 inch box preferred) or greater trees placed 15 feet on center is required (Graphic 8).
- c. Supporting landscape and streetscape improvements shall be incorporated into the streetscape design including decorative paving, lighting, and other landscape elements such as freestanding planters, planter boxes and hanging baskets. Paving must be relatively flat, slip resistant, and of a design that does not create a tripping hazard but adds visual interest and unique imagery. The surface of the sidewalk should appear as a consistent pattern. At the corners, the sidewalk must ramp down to provide for handicapped access. The recent City improvements along the Fourth Street Corridor shall serve as the minimum anticipated streetscape treatment and level of amenity.
- d. Street lights and decorative pedestrian lights shall be incorporated into any future streetscape improvement. Overhead lights must provide the ambient light necessary for safety and traffic operations, and must meet City of Las Vegas Department of Public Works standards. (See specification in Item "f" below.) Pedestrian lights should be placed in a clear geometric pattern with regular spacing to add a sense of order to the street. The design of the lights and the pattern for their placement are part of the overall design for Downtown Las Vegas. The lights reinforce the hierarchal street system; therefore, lights associated with individual development projects are prohibited from being located in the right-of-way.



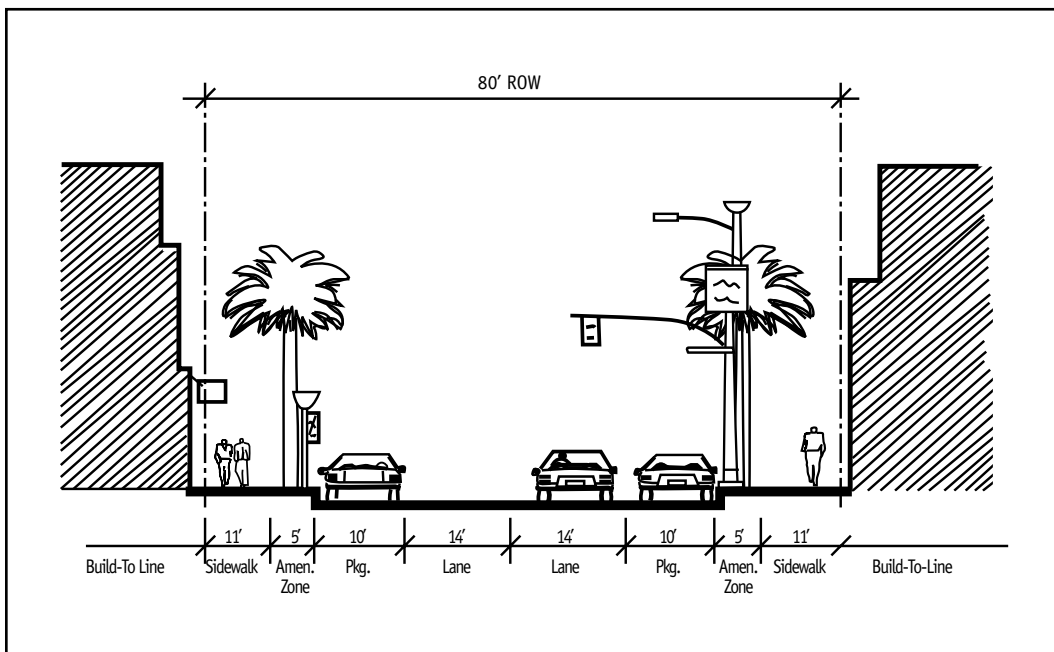
- e. All new streetscapes, sidewalks, and streetscape elements shall conform to match those installed within the Fourth Street Corridor and Las Vegas Boulevard Beautification Projects during 1996 to 1999.
- f. All new developments shall provide and install standard Fourth Street style fixtures in place of existing fixtures. (Exact specifications, shop drawings, and standard suppliers can be obtained from the City of Las Vegas Engineering Design Superintendent, Department of Public Works, 229-6272.)
- g. Landscaping and plant materials shall be as specified in section DS4, Graphic 7 and Graphic 8. (Exact specifications, shop drawings, and standard suppliers can be obtained from the City of Las Vegas Engineering Design Superintendent, Department of Public Works, 229-6272.)



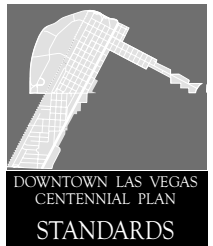
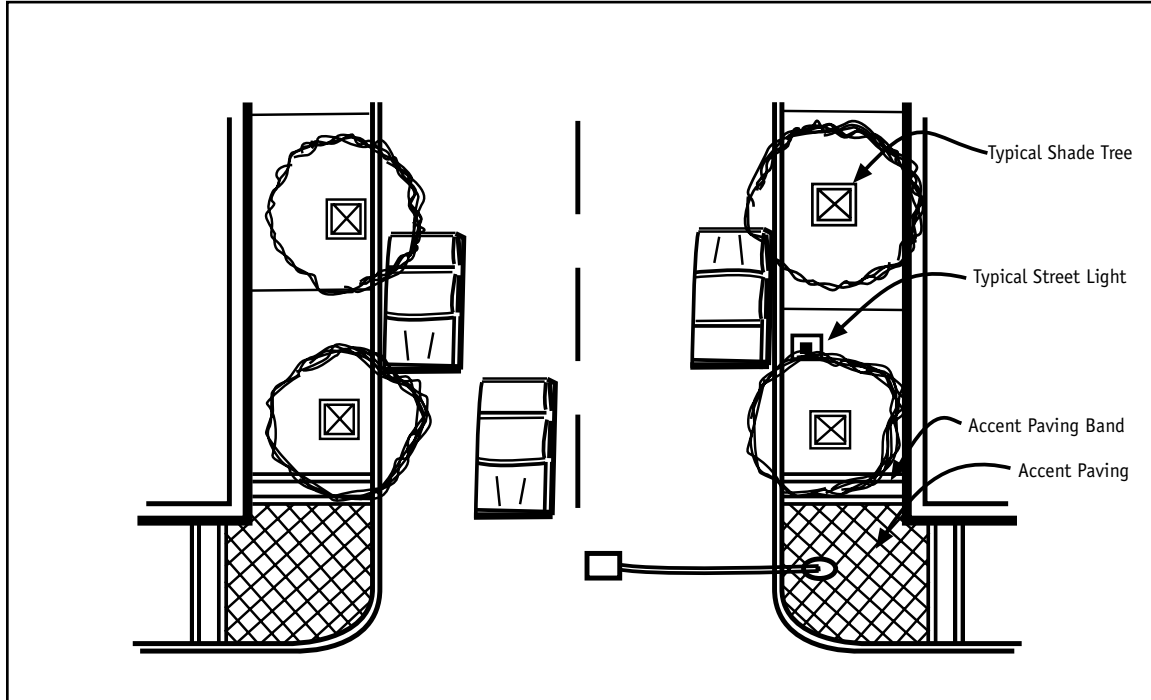
PLAN VIEW



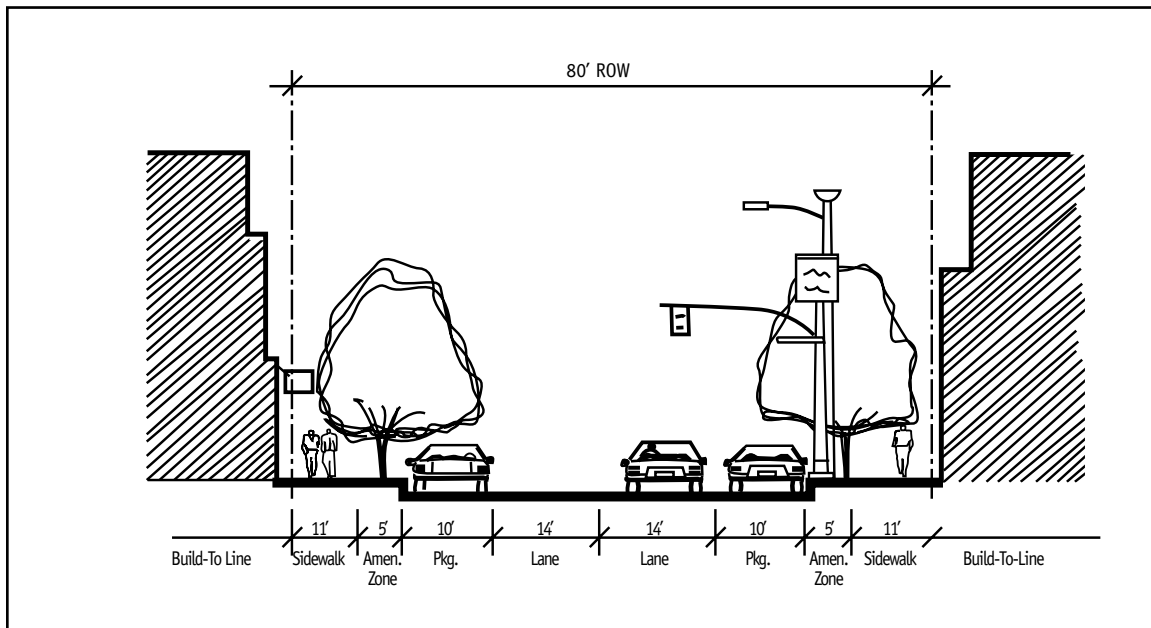
STREET SECTION



PLAN VIEW

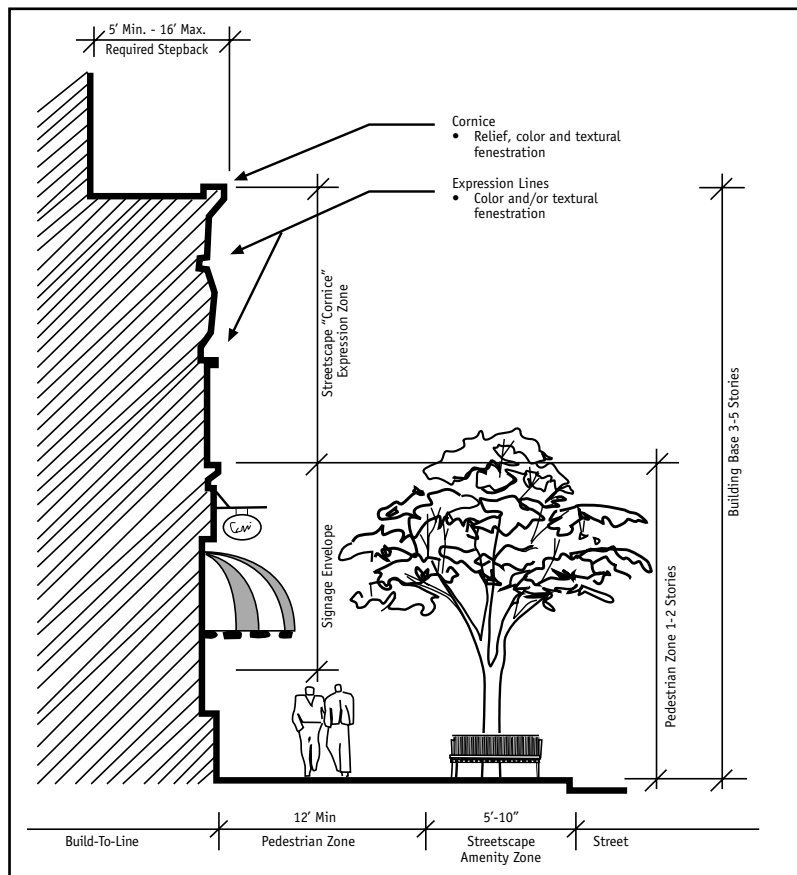
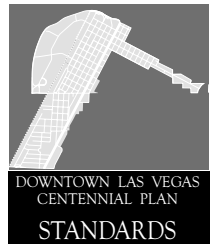


STREET SECTION



## DS5. DOWNTOWN LAS VEGAS ARCHITECTURAL DESIGN STANDARDS

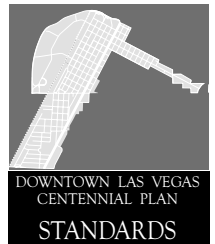
- 1** These are the architectural design standards for all of Downtown Las Vegas:
- Seventy percent (70%) of the building streetwall shall align along the edge of the sidewalk. If immediately adjacent existing buildings are setback, then seventy percent (70%) of the building streetwall may align within the median setback range of existing buildings likely to remain within the same block and street frontage as determined by staff. Setback requirements may be waived by City Council with clear and compelling evidence for buildings that enhance the pedestrian environment through the provision of other pedestrian or civic amenities such as: plazas, courtyards, landscape features, public entries, shade devices, cafes, etc. (Graphic 9).
  - Roofs and rooftops shall be designed with consideration to views from above. No highly reflective materials or contrasting colors shall be used. All mechanical equipment shall be screened from street level and surrounding building views. All obtrusive features shall be minimized.
  - An articulated roof line and / or an articulated cornice shall be designed as a major feature at or near the top of all streetwalls and walls visible from any right-of-way (Graphic 9).



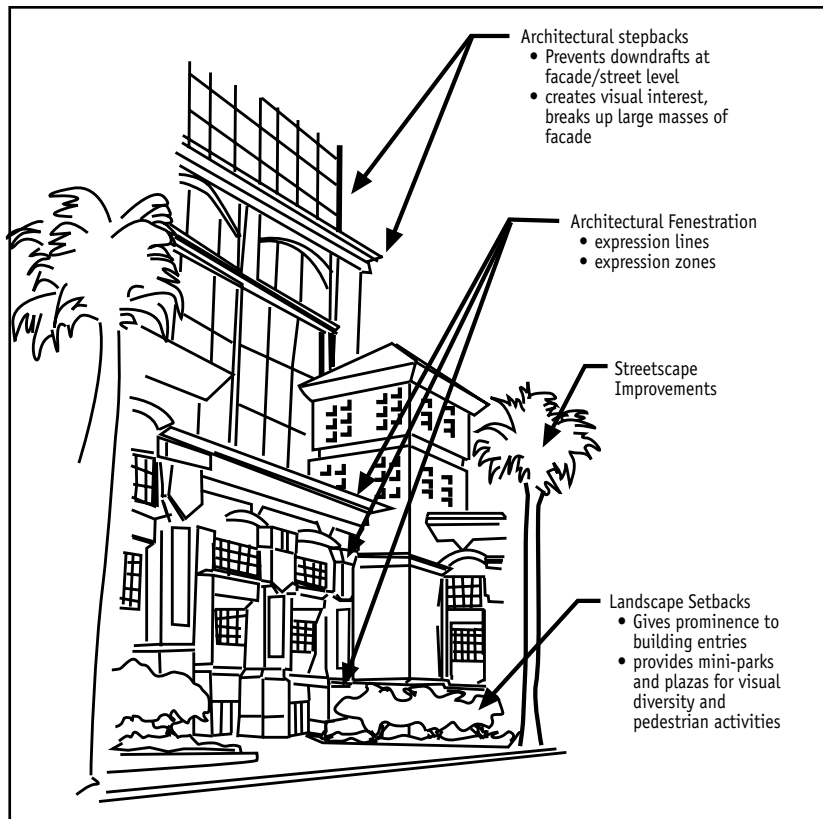
GRAPHIC 9: BUILDING FACADE AT STREET LEVEL



- d. Extended, blank, expressionless walls at the street level shall be prohibited. The use of expression lines and expression zones (that is, areas and features of the facade that incorporate changes in material, color, and / or relief) shall be required in the pedestrian zone to create visually interesting facades (Graphic 10).
- e. The use of arcades, awnings, canopies, and other shade devices is strongly encouraged.
- f. The incorporation of misting systems at the pedestrian level is also strongly encouraged to complement the objectives of this plan.
- g. The main entry of the building from the street shall be appropriately articulated in the architectural design of the building. This shall be accomplished through change in materials, colors, and / or the amount of detailing around the entry; having the entry slightly recessed or protruding from the primary building line; and / or through the use of canopies or awnings, etc. (Graphic 10).
- h. Exterior materials of all buildings shall contribute to a cohesive physical environment, convey a sense of permanence and belonging, and shall be of natural, native materials and/or colors.
- i. Setbacks on streetwalls facing public streets and plazas shall be required to reduce the effects of wind traveling downward along the building face, add visual interest, and promote light penetration at the street level. Setbacks from property lines / sidewalk edges are required at a minimum of 4 stories, 11 stories, and 18 stories in building height. Each setback must be at least five feet in depth, but not greater than 16 feet (Graphic 10).



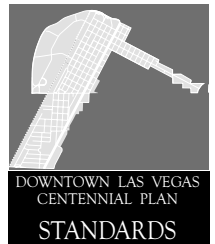
GRAPHIC 10: BUILDING MASSING AND SETBACKS



- j. Service areas (including storage, special equipment, maintenance, and loading areas) shall be screened with landscaping and / or architectural elements from pedestrian or street view. Architectural screening shall be constructed of the same materials and finishes as the primary building.
- k. All service and loading areas shall be positioned so service vehicles will not disrupt traffic flow to or from the site or within parking lots. No service, storage, maintenance, or loading area may extend into a setback area.

## DS6. DOWNTOWN LAS VEGAS SIGNAGE DESIGN STANDARDS

- 1 The design, installation, and maintenance, of all signs shall be in full conformance with current Title 19A.14, Sign Standards, as revised and amended, unless subsequent, more restrictive standards are promulgated and amended hereto.



## DS7. DOWNTOWN LAS VEGAS LANDSCAPE DESIGN STANDARDS

- 1 Landscape design standards shall be as set forth in LVMC 19.06.060.

## DS8. DOWNTOWN LAS VEGAS OUTDOOR DINING STANDARDS

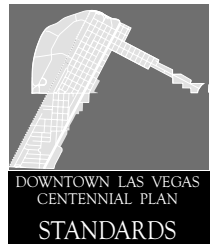
- 1 Applicability. These standards (the "Outdoor Dining Standards") apply to outdoor dining that occurs or is proposed to occur within public sidewalk areas or sidewalk

areas or similar pedestrian areas located:

- a. Within the area depicted on Map 5 as being included within the boundaries of the Downtown Centennial Plan; and
- b. North of Charleston Boulevard and west of Eighth Street.

For the purposes of the Outdoor Dining Standards, the term "outdoor dining" and "outdoor dining area" refer to dining within the area of a public sidewalk or similar pedestrian area, unless the context refers otherwise. The term "similar pedestrian area" refers to an area within public right-of-way that does not qualify as a sidewalk but that is used or intended to be used for pedestrian traffic or circulation.

**2** Objective and Intent. The objective of establishing the Outdoor Dining Standards is to promote pedestrian-friendly use of public sidewalks and similar pedestrian areas in connection with providing economic opportunities for ground floor retail. The Outdoor Dining Standards have been developed to ensure that the space used for outdoor dining is consistent with the general design of the public right-of-way and to allow for adequate pedestrian circulation. The Outdoor Dining Standards are also intended to guide applicants with the design of outdoor dining areas, establish or enhance an identifiable sense of place, create a comfortable and interesting pedestrian environment, maintain a continuous and visible pedestrian activity between nodes and building anchors, and provide minimum standards for beautification.



**3** General Standards. Outdoor dining otherwise permitted within the underlying zoning district may be permitted to take place within a sidewalk area or similar pedestrian area, but only in accordance with the Outdoor Dining Standards.

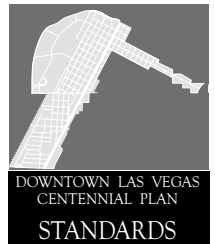
- a. Location.

Outdoor dining may occur only as an accessory use to an eating establishment and may be permitted only in the public sidewalk or similar pedestrian area immediately adjacent to the front of the establishment. The dining area may not extend beyond the boundaries of the abutting property and shall not be located in a manner that interferes with the building egress and ingress as required by the International Building Code. No outdoor dining area may be located within twenty feet of an intersection or within ten feet of a driveway or alley. Where approved by the Department of Public Works, outdoor dining in a public sidewalk or similar pedestrian area may occupy up to two thirds of the total width of the sidewalk or available pedestrian area. In the case of a sidewalk or similar pedestrian area up to fifteen feet wide, there shall remain a minimum pedestrian clearance of six feet. For sidewalks or similar pedestrian areas with a greater width, the minimum pedestrian clearance is eight feet. This minimum clearance

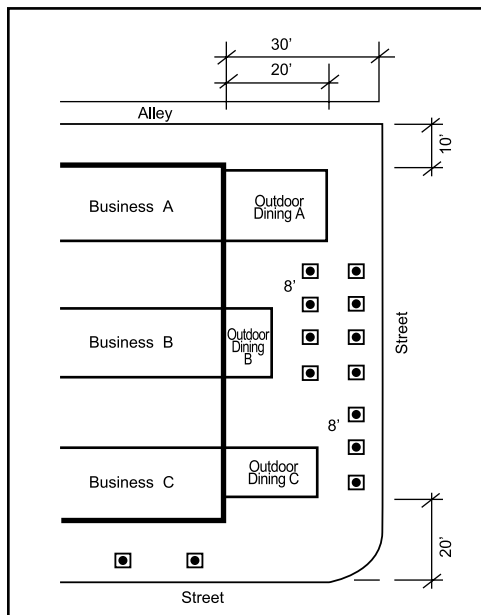
area must occur between any outdoor dining and a continuous line that represents where sidewalk or pedestrian area obstructions are located. Such obstructions include without limitation tree planters, landscape planters, street furniture, streetlight poles, utility poles, fire hydrants, signposts, and permitted news racks. (See Graphic 11)

**b. Barriers.**

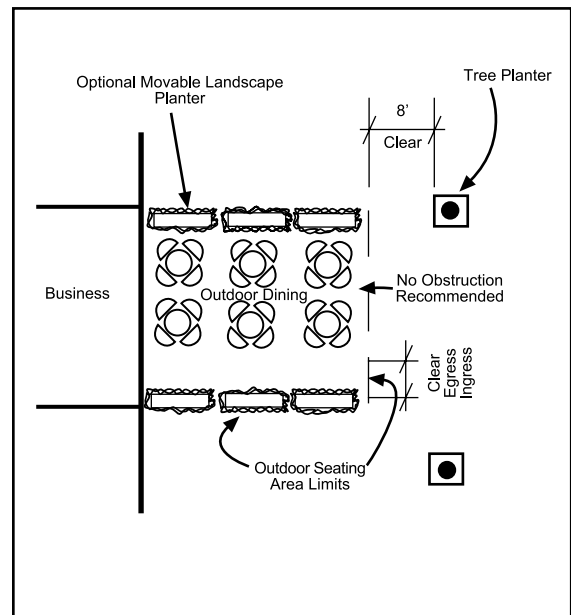
Outdoor dining areas shall be free of any permanent barrier or other structure that creates physical separation between the dining area and areas of public sidewalk or a similar pedestrian area that are not used for outdoor dining. However, the outdoor dining area may be delineated by the placement of temporary barriers such as balustrades, cordons, railings or landscaping. Such barriers shall be no more than thirty-six inches in height and shall be entirely portable. Landscape planters mounted on rollers are the recommended option. No height limitation shall apply to the landscaping, but all dividers and barriers shall be designed to be in keeping with the aesthetics and architectural character of the area and the abutting building and business. Obstructions that are designed to define the outermost edge of the outdoor dining area and that face the area of open pedestrian circulation are strongly discouraged in order to preserve views and maintain an inviting movement of patrons. No barrier may be affixed to the sidewalk or other permanent means of attachment. The approval of an encroachment permit or agreement for outdoor dining does not authorize any damage or modification to the texture of the surface of public sidewalk or other pedestrian area, and the holder of such a permit or agreement shall not cause any such damage or modification. Rubberized footings shall be used as necessary to avoid damage to the sidewalk surface. No item of furnishings, including tables, chairs, umbrellas, planters, host podium, lighting and related furnishings and equipment, shall be attached to a sidewalk or sidewalk surface, nor shall they cause damage to the sidewalk in any manner (see Graphic 12).



GRAPHIC 11: OUTDOOR DINING LOCATION



GRAPHIC 12: OUTDOOR DINING BARRIERS



c. Roofing.

An outdoor dining area shall remain open to the air, with no permanent covering over any part thereof; provided, however, that such an area may be covered by a building articulation that is an integral part of the architectural design and that is consistent along the entire ground floor frontage of the building, such as an arcade or a fixed canopy. A misting system may be incorporated along the exterior face of the building and shall be architecturally integrated with the building articulation. Additional shade may be provided over the outdoor dining area by means of potted landscaping, umbrellas or awnings. Umbrella canopies shall not be lower than 7'-0", shall be located so as not to encroach beyond the boundaries of the approved dining area, and shall not support any printed product advertisement. Awnings shall be adequately secured to the face of the building without any supports extending to the sidewalk surface and shall be fully retractable. The awnings shall be rolled up at the end of each business day, and their design shall complement the architectural features and color of the building.

d. Furniture.

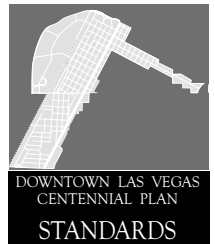
All furnishings within an outdoor dining area shall be movable and made of sturdy, durable and commercial grade material. They shall be designed to complement the design theme of the business. Ordinary plastic lawn chairs and tables and similar furniture are not acceptable.

e. Trash Enclosures.

Trash enclosures or receptacles for the use of the eating establishment and its customers are not permitted within the outdoor dining area or the adjacent public sidewalk or similar pedestrian area.

f. Lighting.

Lighting, when provided, shall be shielded and of low wattage so as to illuminate only the outdoor dining area and so as to avoid producing glare that has a negative impact on pedestrian traffic. The design of the light fixtures shall be compatible with the architectural theme of the building and business.



## 4 Standards of Operation.

a. Storage of Furniture and Equipment.

All outdoor furnishings related to an outdoor dining area, and all equipment used to identify or delineate the dining area, may remain within the sidewalk or similar pedestrian area only during the hours of operation of the eating establishment. All such furnishings and equipment must be removed at the end of each business day, with the sidewalk or similar pedestrian area remaining clear of obstructions.

b. Maintenance.

Outdoor dining areas shall be kept in good state of repair and maintained in a clean, safe and sanitary condition. Any item of furniture or equipment that is broken, rusting, degraded, torn, or tattered shall be removed promptly. The operator of the establishment shall be responsible for removing trash and debris within the outdoor dining area, as well as within an area extending twenty feet in each direction from the outermost boundary of the outdoor dining area. The operator must also clean (including sweep-

ing and mopping) the area in and around the outdoor dining area at the end of each business day.

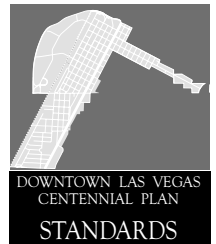
**c. Music or Live Entertainment.**

Music or live entertainment is an amenity with the potential to enhance the experience of outdoor dining area and thereby draw patrons to the establishment. Therefore, an establishment may provide music or live entertainment in conjunction with the food service when all the following criteria are met:

- 1) The music or entertainment is provided only in dining areas.
- 2) Sit-down meal service is provided at all times while the music or entertainment takes place.
- 3) There is no dance floor or dancing.
- 4) There is no cover charge or minimum drink purchase requirement.
- 5) The music or entertainment does not conflict with any established street performance program for the area.
- 6) The music or entertainment complies with applicable noise ordinances and standards.

**d. Alcoholic Beverages.**

Alcoholic beverages may be served in an outdoor dining area only in connection with the operation of a Restaurant Service Bar. In addition to any condition or limitation imposed in connection with the approval of a Special Use Permit, there shall be no visible posted advertising of alcoholic beverage or alcoholic beverage service in any outdoor dining area. A non-freestanding, closable, book-style beverage menu may be permitted at tables in conjunction with a food menu.



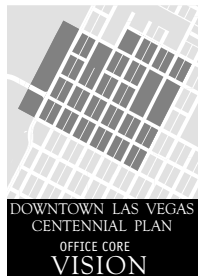
**5** **Zoning Approval Process.** In order to qualify for the approval of any encroachment permit or agreement that may be necessary to operate outdoor dining, the applicant must first obtain approval of a site development plan in accordance with the provisions below.

- a. Except as provided below, the approval process under Title 19 for outdoor dining is that of a Minor Review of Site Development Plan pursuant to LVMC 19.18.050(F).
- b. Following a required pre-application conference, a committee shall review the proposal the committee shall be comprised of:
  - 1) The Director of Planning and Development or a designee;
  - 2) The Director of Public Works or a designee; and
  - 3) The Director of the Office of Business Development or a designee.
- c. The committee may approve an application for outdoor dining administratively if the committee finds that the proposal is consistent with the Downtown Centennial Plan and complies with the Outdoor Dining Standards. Approval by the committee may be conditioned upon the subsequent approval of an encroachment permit or agreement. The committee, in its sole discretion, may also impose additional conditions upon the approval in order to further the goals and objectives of these Standards. Locations approved for outdoor dining shall be subject to both ongoing and complaint-based inspections. In accordance with LVMC 19.18.050(I), approval, and noncompliance may form the basis of non renewal or revocation of a business license.

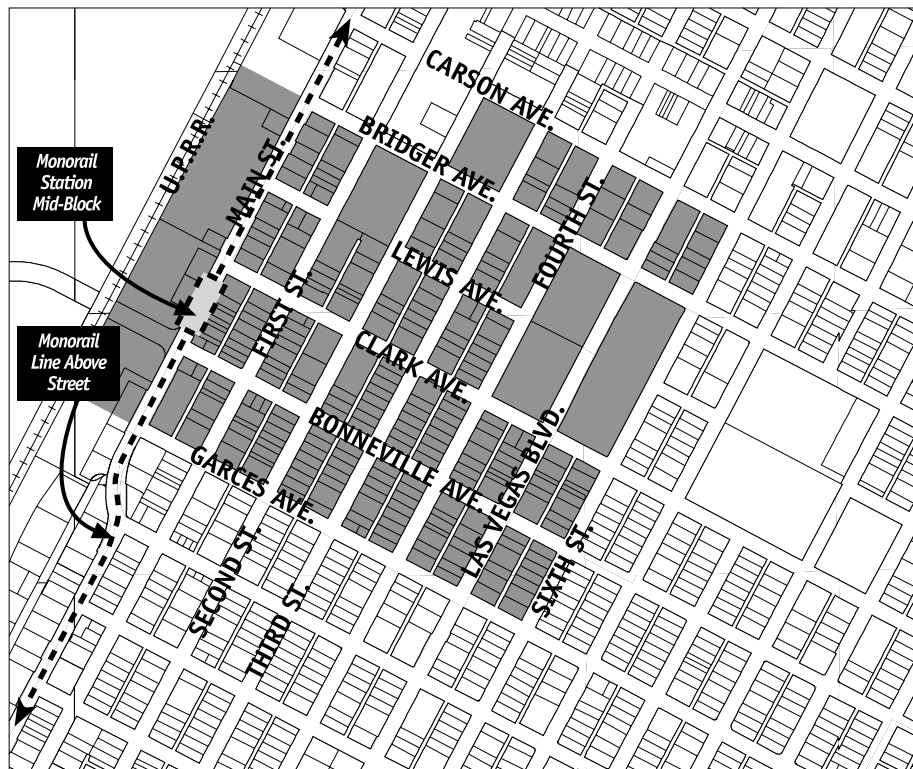
## OFFICE CORE DISTRICT: VISION

### V5. OFFICE CORE: BOUNDARIES

**1** The Office Core District shall be defined as the area bound by the eastern edge of the Union-Pacific Railroad right-of-way on the west, the centerline of Bridger Avenue between the tracks, and Casino Center Drive, but then the centerline of Carson Avenue between Casino Center Drive and Sixth Street on the north, on the east the centerline of Sixth Street, and the centerline of Garces Avenue on the south (Map 8).



MAP 8: OFFICE CORE: DISTRICT BOUNDARY

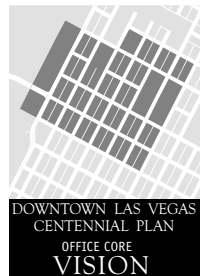


#### V5A. OFFICE CORE: EXISTING CONDITIONS

1 The Office Core District is composed of a mix of uses; including corporate office towers; Clark County government offices and court complex: a Federal Courts complex; an historic school now being used for commercial and governmental office space; a detention center; small hotels, churches, and assorted residential structures.

#### V5B. OFFICE CORE: MARKET DEMAND

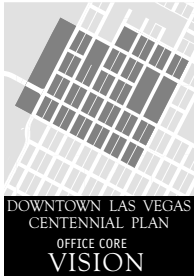
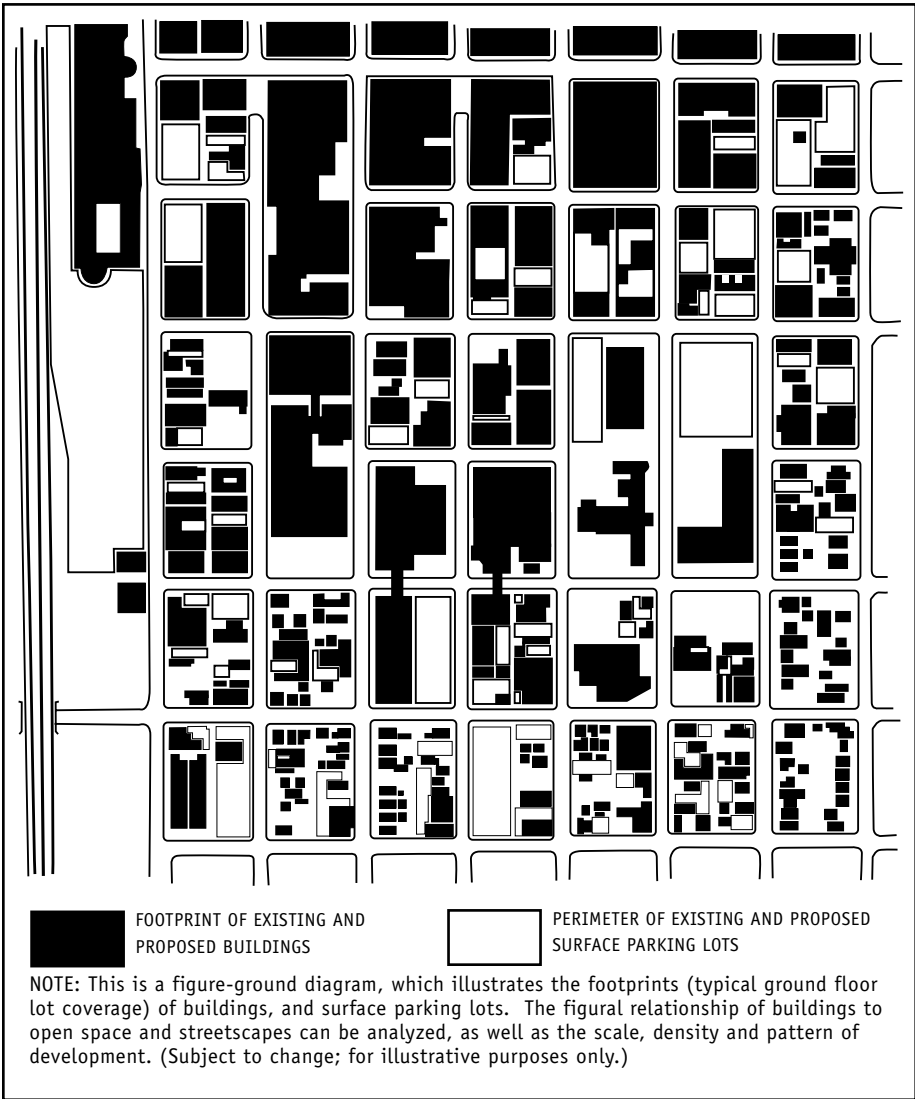
1 Until recently, Downtown Las Vegas and the Office Core District has not seen any significant new office construction (over 50,000 SF) since 1986. According to real estate development advisors, Keyser Marston Associates, Inc., there is ample market support for downtown office development in the future (up to 330,000 SF by 2001, an additional 350,000 SF to 2006). With the correct development strategies employed, Downtown Las Vegas could expect to capture all that demand and more.





1 Existing development patterns in the Office Core illustrate the historic land ownership of small, individual parcels mixed throughout the area. A few blocks have been redeveloped beyond those existing patterns, and current construction projects and development proposals will alter another seven or so blocks. The development patterns in the southern portions of the Office Core still reflect the small, historic ownership patterns while the northern portions of the Office Core illustrate the more recent development patterns showing larger buildings on larger parcels (refer to Map 9 for an illustration of existing development patterns).

MAP 9: OFFICE CORE: EXISTING URBAN FORM PLAN WITHIN AND SURROUNDING THE OFFICE CORE DISTRICT



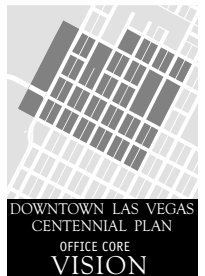
**2** The small parcel size and individual ownership patterns in the Office Core create a difficult scenario in which large-scale redevelopment efforts can be successful. It is difficult to find parcels of adequate size or to assemble parcels of adequate size for development of a quarter to a half block in size or larger. Many existing land owners are holding onto properties that they mistakenly believe have a potential speculative value for casino development, which is significantly higher than for office development. This fact, combined with numerous property ownerships, creates a very difficult situation for any future office development to occur in the Office Core. Clear and revised delineation of the Gaming Enterprise Overlay would redirect land values within and surrounding the Office Core District.

**3** The City should work with the Clark County Assessor to determine the appropriate and fair assessments of parcels and improvements within the downtown area, increasing assessments to at least 90 percent of current market value, or comparable. This will encourage divestiture of under utilized parcels and assist with assembly of small lots into useful redevelopment sites.

**4** It is the intention and goal of the City to encourage the development of new complementary urban development within the Office Core District. Desirable unifying elements can include building design, size, shape, mass, height, color, and materials. This will, over the next generation, overcome the seemingly random pattern of development that has occurred previously in the Office Core District.

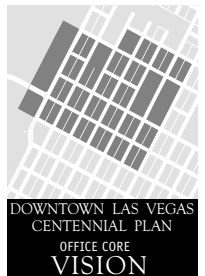
**5** There are currently few amenities for the pedestrian in the Office Core. The recent completion of the Fremont Street Experience and the Fourth Street Corridor landscape improvements have added great value to what was a generally harsh pedestrian experience. The Office Core lacks parks, open space for public gatherings or lunch-time activities, trees, benches, and attractive surface treatments for walkways.

**6** The Office Core does not currently boast a great diversity of uses. Most current uses consist of offices oriented toward the courts and judicial activities. Small support uses such as copy centers, banks, and other similar uses are found sporadically throughout the denser development areas. It is the intention and goal of the City to encourage new class A professional office development, a rich variety of retail and commercial storefronts, restaurants and cafes, mixed-use and residential developments within the Office Core District.



**1** The existing street system in the Office Core has adequate capacity to accommodate the anticipated future demands of potential new development. There are some capacity constraints in specific segments and at specific points and some lack of network continuity to address. For many years, there has been no consistent zoning requirement for parking in the Office Core. Existing parking on streets, surface lots, and in garages is at practical capacity with little or no room to absorb future large scale development parking needs. Little attention has been given toward transportation management strategies such as demand reduction through ride-sharing and other programs. Bus turnouts along all bus routes, but especially Las Vegas Boulevard, are strongly encouraged where right-of-way acquisition permits.

**2** Consideration should be made for a “park-and-ride” facility on one of the parcels adjacent to the monorail line on Main Street between Clark Avenue and Bonneville Avenue. The park-and-ride facility could serve both the Bonneville Station and the Fremont Station. Users of the park-and-ride would then be able to enjoy the Resort Corridor without the worry of finding parking around Fremont Street or along the Corridor, while avoiding the congestion of Las Vegas Boulevard.

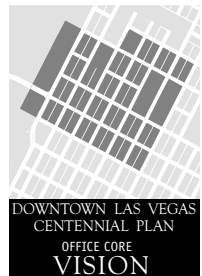


## V5E1. OFFICE CORE: MARKET STRATEGIES

- a. Assemble large development parcels suitable for large-scale redevelopment projects.
- b. Establish amenities for Office Core tenants, workers, and visitors. Provide ample, affordable parking, lighting, landscaping, parks / open space, and encourage support uses such as restaurants and retail.
- c. Casino development shall be limited to two finite casino areas outside the Office Core: Fremont Street Experience / Casino Center, and The North Strip area along Las Vegas Boulevard between Sahara and Charleston, pursuant to the revised Casino Enterprise Overlay District. Target future casino development for these two distinct districts.

## V5E2. OFFICE CORE: LAND USE AND URBAN DESIGN STRATEGIES

- a. Parkway Center is a Planned Development (PD) zoned District requiring a master development plan and design standards. The master development plan and standards will establish appropriate land uses, minimum and maximum Floor Area Ratios (FAR's), parking strategies, open space, streetscapes, and relationships to the Downtown Las Vegas area east of the site.
- b. Establish a system of arcades within and beyond the Office Core, recapturing a sense of the City's historic past, reinvigorating the streetscape, providing comfort for pedestrians, and creating a unique and identifiable image.
- c. Establish prominent civic squares or parks within or near the Office Core. A park or square could be combined with the possible construction of a new City Hall.
- d. Establish one or more residential neighborhoods at the edge of the Office Core, which will induce related shopping, recreation, restaurant, and service uses in the area, including a 24-hour lifestyle.



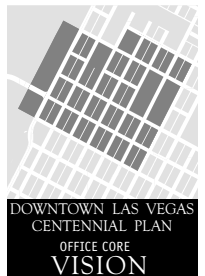
## V5E3. OFFICE CORE: TRANSPORTATION STRATEGIES

- a. Complete the second half of the paired one-way couplets on Casino Center southbound beginning south of Bridger Avenue, perhaps at Clark Avenue.
- b. Connect Main Street with Industrial Road to create a back road service route for the casinos.
- c. Explore the potential to connect St. Louis Avenue with Oakey Boulevard.
- d. Implement signage improvements, both freeway and internal.
- e. Upgrade existing signal system and add new signals at specified intersections.
- f. Limit any further street closures in the Office Core and surrounding area.
- g. Establish a CCDC and / or City parking strategy and unified Downtown Parking Plan to develop a system for the unified provision and management of centralized off-street and on-street parking.
- h. Expand alternative modes of transit for long-term benefits. This includes:
  - 1. Designating Main Street for a preferred fixed guideway alignment corridor
  - 2. Developing an intermodal transportation center
  - 3. Promoting incentives for transportation reduction including ride-sharing, pedestrian systems, preferred ride-share parking, etc.
- i. Targeting the visitor market via future transit investments

**1** The Urban Design Principles provide a framework and graphics for the Office Core in order to achieve the ultimate goal of repositioning Downtown Las Vegas as an attractive, safe, and economically vital regional center for business, commerce, government, entertainment, and the arts.

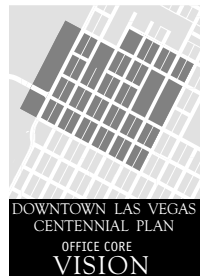
### V6A. OFFICE CORE: URBAN FORM PRINCIPLES

- a. Respond to the overall urban structure and infrastructure to the extent possible. Realignment of streets, street closures, or other significant infrastructure changes shall be discouraged.
- b. Strive to seek large-scale redevelopment opportunities (quarter to half block in size or larger) in the Office Core of Sixth Street west through consolidation of the historically small development parcels and ownership patterns.
- c. Minimize large “gaps” in the urban fabric as a result of surface parking lots or underutilized parcels. Maximizing the available land through consolidated redevelopment efforts is preferred over the random, scattered, small-scale patterns that have historically taken precedent.

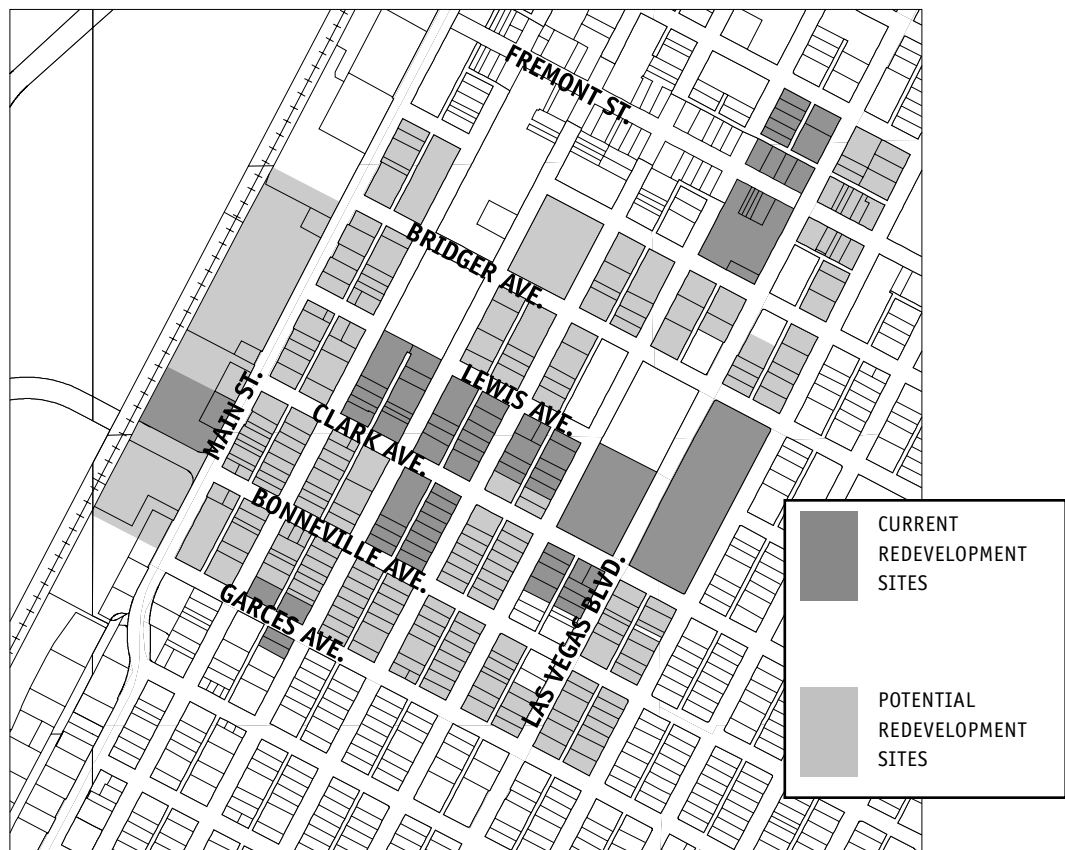


**1** The Office Core Potential Redevelopment Areas Plan targets those areas in the Office Core which should become the focus for future redevelopment efforts (Map 10). The blocks highlighted generally represent redevelopment potential because they meet any one or all of the following conditions:

- a. Comprised of historically small parcelization and land ownership patterns currently represented by small structures or vacant lots.
- b. Hosting structures which have reached a point where they are no longer competitive in the Class A office market.
- c. Properties that demonstrate a need for redevelopment uses that support a synergistic relationship to the Office Core District (i.e. business restaurants, hotels, entertainment venues, etc.)
- d. Hosting structures that lack historical or culturally significant value.



MAP 10: OFFICE CORE: POTENTIAL REDEVELOPMENT AREAS PLAN WITHIN AND SURROUNDING THE OFFICE CORE DISTRICT



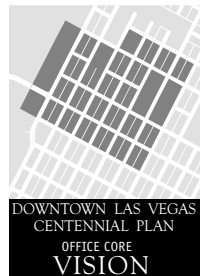
**1** Pursuant to Title 19, City of Las Vegas Zoning code, as amended, the Office Core District is primarily zoned C-2, General Commercial, which allows a wide variety of commercial uses. Some parcels are now zoned C-V, Civic, which allows particular civic uses such as courthouses, public offices and the detention center. A small number of parcels may still be zoned C-1, Limited Commercial, and R-4, High Density Residential, which reflects the existing land uses of those parcels.

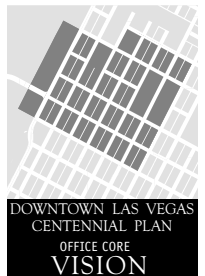
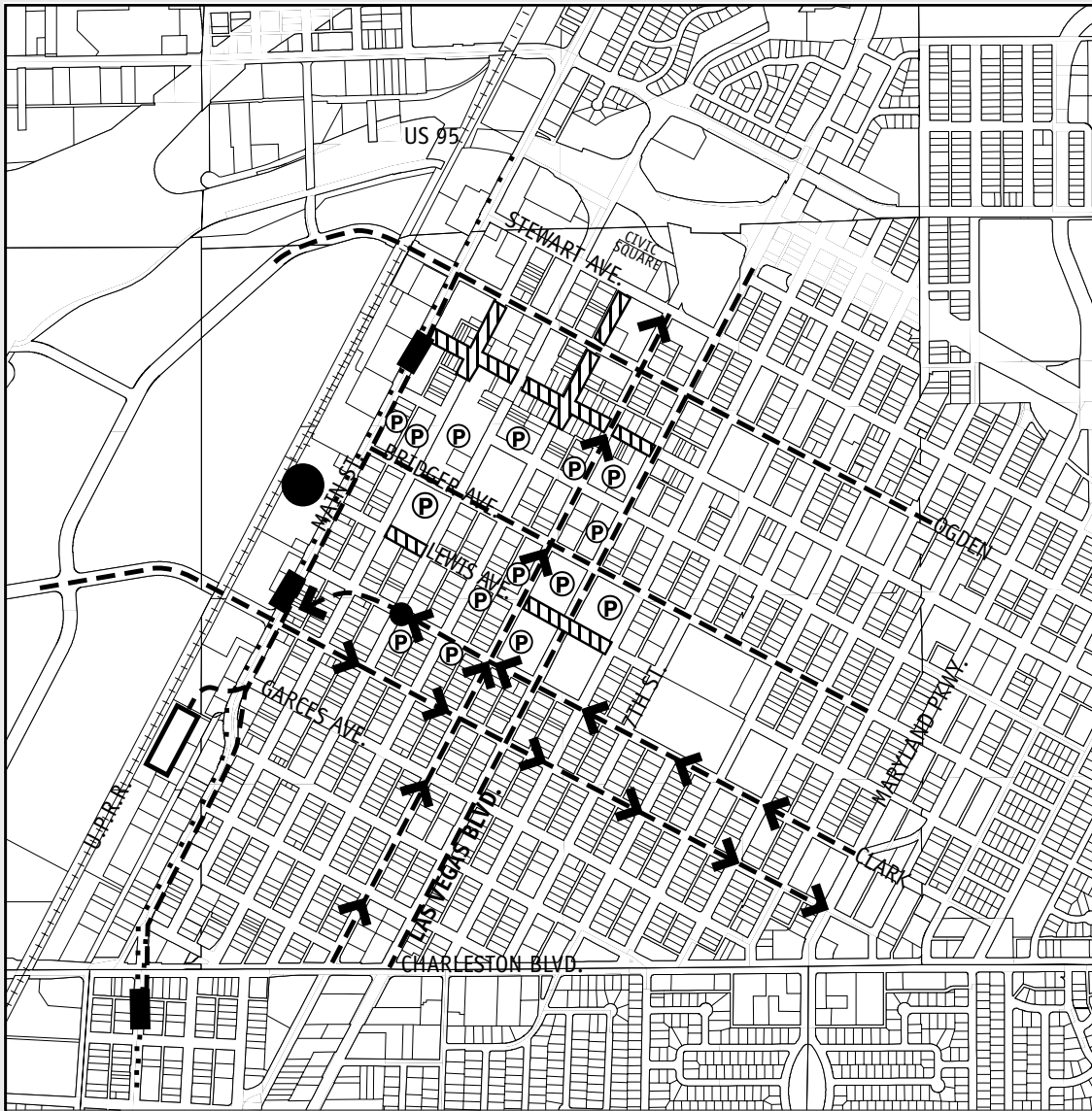
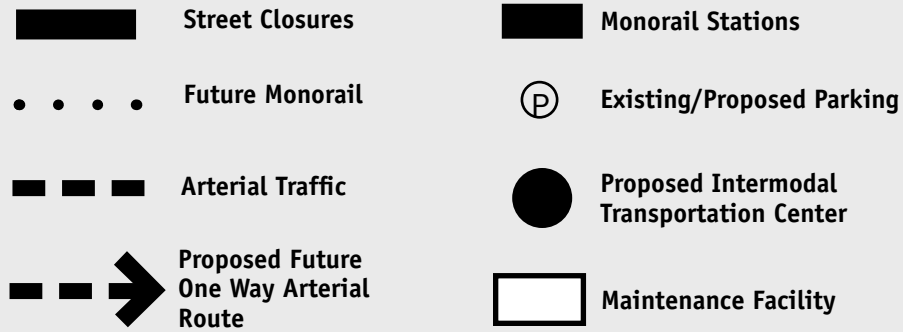
**2** As redevelopment occurs, it will be appropriate to rezone sites for commercial use to C-2 and sites for public facilities to C-V. It is the intention of this Plan to allow the entire Office Core District to redevelop as commercial office and mixed-use development, with the possible incorporation of very high density residential uses.

#### V6D. OFFICE CORE: VEHICULAR TRANSPORTATION / ACCESS

**1** The Office Core Transportation / Access Plan (Map 11) illustrates the primary vehicular network considerations for the Office Core. Of particular note are the following strategies:

- a. Keep street closures to a minimum where possible.
- b. Incorporate the proposed fixed guideway transit system along Main Street with stations along Main at Fremont Street Experience and Clark Street.
- c. Complete the north-south one-way paired couplet system in the Office Core and throughout Downtown with Fourth St. being one-way northbound and Casino Center Boulevard being one-way southbound beginning at Bridger Avenue.
- d. With Alta Avenue widening planned to the west of Downtown, consider developing an east-west one-way paired couplet system in the Office Core and beyond, with Bonneville Avenue becoming one-way eastbound and Clark Avenue becoming one-way westbound. The block bound by Main, Clark, First and Bonneville would have to be redeveloped to allow Clark to transition toward the new Bonneville underpass.
- e. Establish shared parking structures to provide the opportunity of common shared facilities that generate pedestrian movement between parking and land uses.
- f. Establish a City Parking Authority to provide a unified supply and management of on-street and off-street parking in the Office Core District and throughout Downtown. The Parking Authority's responsibility would include planning, design, construction, financing, land acquisition, operation, monitoring, and enforcement of all public parking in Downtown Las Vegas.



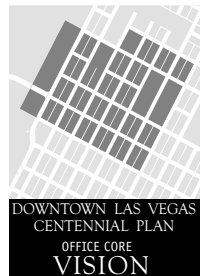


MAP 11: OFFICE CORE: TRANSPORTATION / ACCESS WITHIN AND SURROUNDING THE OFFICE CORE DISTRICT



**1** The Office Core Primary Pedestrian System Plan (Map 12) illustrates a hierarchy of pedestrian improvements to be established in the Downtown Las Vegas area. It will be particularly important to:

- a. Establish Lewis Avenue as a primary east-west pedestrian corridor in the Office Core linking the Union Pacific site on the west to the old high school on the east. The Lewis Avenue Corridor should have distinctive paving treatment, street widths, landscaping, lighting, and signage along its length in the Office Core. It should also incorporate a significant civic plaza at Third Street in conjunction with proposed developments.
- b. Extend the landscaped median treatment on Las Vegas Boulevard from the Strip to its intersection with Interstate 515.
- c. Establish Third Street as a primary north-south pedestrian linkage in the Office Core. It will perhaps have a landscaped median or widened landscaped sidewalks and distinctive lighting, signage, and paving. It will serve primarily as a pedestrian conduit for movement from the Lewis Avenue Corridor north to the Fremont Street Experience and southward into new residential and mixed-use neighborhoods.
- d. Maintain Fourth Street as an enhanced pedestrian and vehicular route through the Office Core. It will continue as a one-way northbound vehicular connector with its own landscaping, lighting, and special paving treatments.
- e. Establish an overall landscape theme for the Office Core consisting of north-south streets lined with palm trees and supporting landscape elements, and east-west streets lined with shade trees and supporting landscape elements.
- f. Establish pedestrian connections over / under the Union Pacific railroad tracks linking the Office Core District with the Downtown West District and future development activities.



## V6F. OFFICE CORE: PARCEL BUILD-TO LINES AND STREETWALLS

**1** The Office Core Streetwall and Parcel Build-To Lines (Map 13) strengthen the primary pedestrian network by requiring that buildings contribute to the sense of place through consistent massing, building heights, and their frontage along pedestrian corridors at the sidewalk and street edges.

**2** The parcel build-to-lines are designated along the primary pedestrian corridors along Lewis Avenue, Bridger Avenue, Clark Avenue, Third Street and Las Vegas Boulevard. These are designated such that building facades will help define the street and that uses at the ground floor will contribute to animating the street through pedestrian activity. Minimum parcel sizes and Floor Area Ratios determine the bulk of a potential structure ensuring that small, singular uses do not continue to dominate the Office Core. Seventy percent (70%) of the building mass facing the streets and entries should abut

the build-to line. Minor variations in facade articulation for columns, pillars, etc., will be allowed and applied toward abutting requirements. The build-to line pertains only to the first three stories or 45 feet, whichever is greater, of the building.

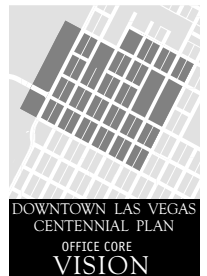
**3** Structures shall have a consistent relationship to one another. A consistent streetwall encourages pedestrian movement. The consistency in urban form will help delineate and define the overall urban character of the Office Core and strengthen the visual imagery. The areas not delineated for setbacks, building height, or Floor Area Ratio (FAR) requirements may have design freedom in accordance with other design standards for the Office Core outlined later in this document.

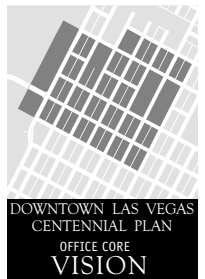
#### V6H. OFFICE CORE: GROUND LEVEL USES

**1** The Office Core Ground Floor Special Use Plan (Map 14) illustrates those areas that must have standards for use at the ground level to contribute to activity at the street level.

**2** Fremont Street, Bridger Avenue, and Clark Avenue have been identified as the primary east-west commercial streets, because of the potential for connectivity to the Downtown West District and the proposed location of Fixed Guideway Stations along Main Street at Fremont and Clark. Main Street, Third Street and Las Vegas Boulevard are identified as the primary north-south commercial streets.

**3** In the areas designated Retail / Commercial, seventy percent (70%) of the building at the street level should have uses oriented toward retail, entertainment, and / or commercial activities. This will provide for uses that will generate pedestrian activity as well as night-time activity generated by restaurants and entertainment venues. To provide interaction between the building and the street, penetrations via doorways, porticoes, or other openings into the building from the street must occur a minimum of every 75 feet of facade fronting the street or plaza areas in this zone.





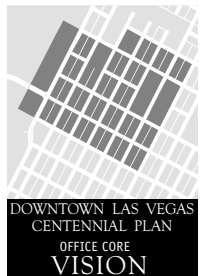
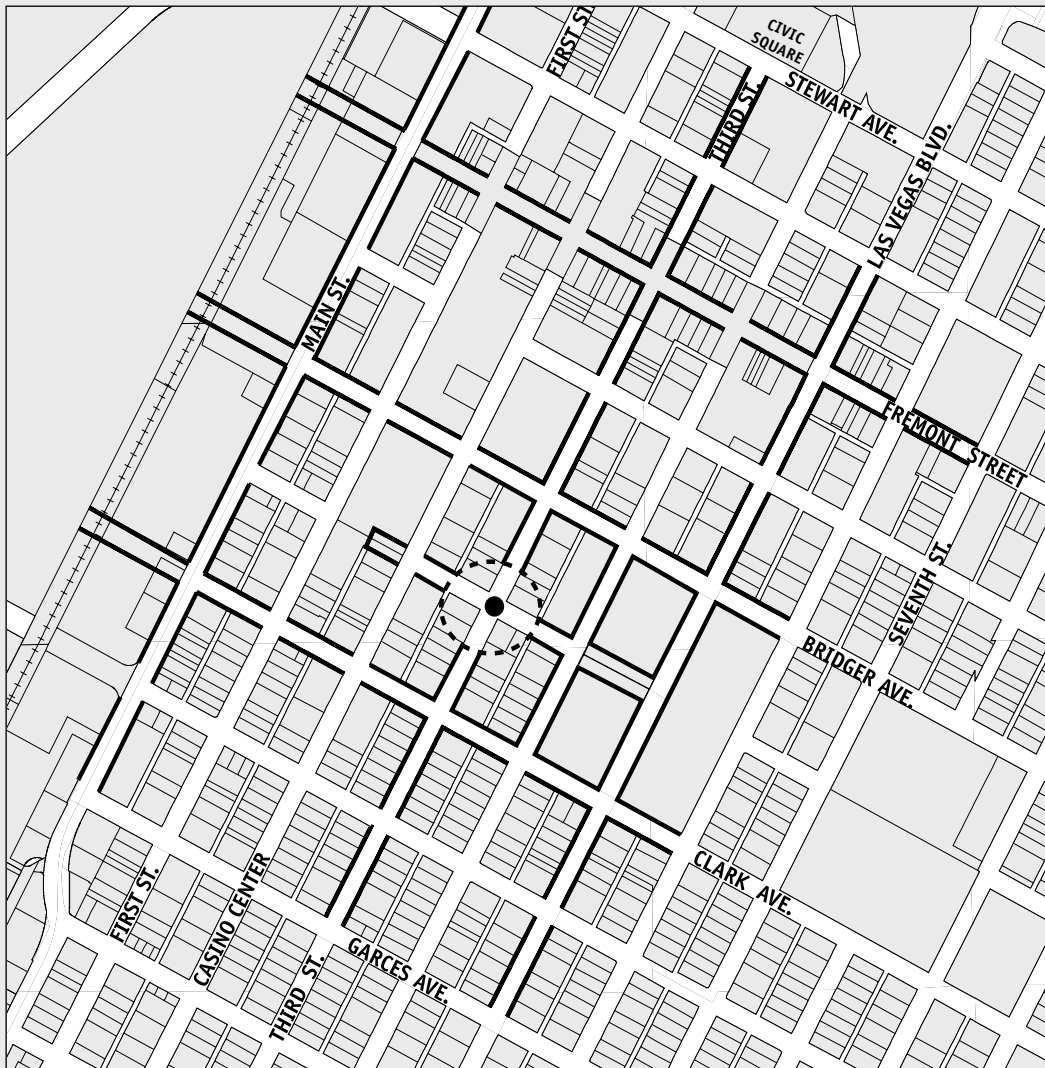
MAP 12: OFFICE CORE: PRIMARY PEDESTRIAN SYSTEM WITHIN AND SURROUNDING THE OFFICE CORE DISTRICT



Recommended Build-To Line  
(at Property Line)

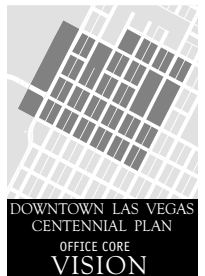


Recommended  
Special Pedestrian Features



MAP 13: OFFICE CORE: STREETWALL AND PARCEL BUILD-TO LINES WITHIN AND SURROUNDING THE OFFICE CORE DISTRICT

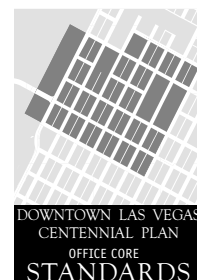
-  Retail / Commercial Frontage Desired at Sidewalk
-  Fremont Street Experience Corridor
-  Neonopolis, Urban Destination Entertainment Center
-  Fifth Street School Potential Re-Use



MAP 14: OFFICE CORE: GROUND FLOOR SPECIAL USE REQUIREMENTS WITHIN AND SURROUNDING THE OFFICE CORE DISTRICT

## OC. OFFICE CORE DISTRICT: DESIGN STANDARDS

**1** The following Office Core Design Standards apply only to the Office Core District. Those standards that are more restrictive than the standards for all other districts appear in bold below. The purpose and intent of establishing Office Core design standards is to give overall direction to future public and private improvements throughout the district.

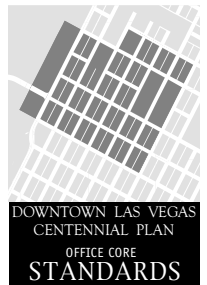


### OC1. OFFICE CORE: SITE PLANNING STANDARDS

- 1** These are the site planning standards for the Office Core District:
- a. **LAND USES:** The following land uses are encouraged within the Office Core District: Office, Office Related, Office Support, Retail, General Commercial, Service Commercial, Restaurants, Cafes, Mixed-Use Developments, Entertainment, and High-Density Residential. Other allowable uses pursuant to Title 19, City of Las Vegas Zoning Code, as revised, and the City of Las Vegas General Plan, are permissible.
  - b. **BUILDING HEIGHT:** All development proposals for new buildings and structures within the Office Core District shall conform to a minimum building height of two stories unless waived by the City Council for demonstrated economic hardship.
  - c. **FLOOR AREA RATIO:** All development proposals for new buildings and structures within the Office Core District shall conform to a minimum Floor Area Ratio (FAR) requirements, which shall be 1.5 or greater, unless waived by the City Council for demonstrated economic hardship.
  - d. **LOT COVERAGE:** Within the Office Core District, lot coverage may range up to a maximum of 100 percent. No minimum lot coverage has been established.

**2** All of the following site planning standards for the Office Core District also pertain to Downtown Las Vegas as a whole:

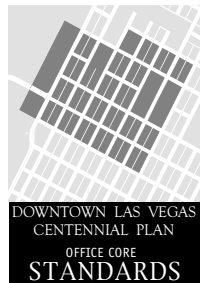
- a. Plan all structures to conceal service areas and storage areas from public street view. Incorporate these uses into the main building whenever possible.
- b. Orient and screen all auto-related facilities (working bays, storage, etc.) from public view. When parking lots face major public streets, incorporate ornamental screen and landscaping. (see Graphic 5.)
- c. Buildings shall orient their public entrances toward major street frontages as much as possible, particularly on major pedestrian streets.
- d. Utilities and power service lines shall be located underground for all new development. To the extent possible, relocation of utilities from alleys (above ground) to streets (below ground) shall be undertaken with all new substantial development activities.
- e. When alleyways are abandoned and building envelopes expanded to create larger development parcels and eliminate negative character of existing alleys, the developer shall relocate utilities and design sidewalks to meet the approved design. If the alley is not abandoned then it shall conform to the standard Alleyway Treatment (Graphic 4).



## OC2. OFFICE CORE: TRANSPORTATION AND PARKING STANDARDS

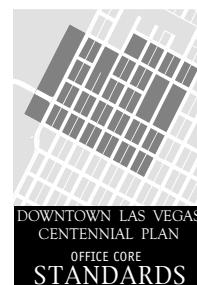
**1** All of the following transportation and parking standards for the Office Core District also pertain to Downtown Las Vegas as a whole:

- a. Any on-street parking shall not count in meeting parking spaces as required. Limited, on-street parking may be provided as a reserve of short-term parking or for service and delivery purposes.
- b. All off-street surface lots shall be paved and include access aisles and driveways. Surface treatments including paving and striping shall be maintained in good condition at all times.
- c. Whenever a driveway is abandoned, the owner shall remove all driveway pavement and approach, re-install curb and gutter, and landscape the area to match the adjacent landscaping / streetscape.
- d. Parking areas shall be separated from buildings through surface treatment variations, such as landscape buffers, and / or by grade separation. Parking spaces that directly abut buildings shall be prohibited.
- e. Wherever possible, adjacent parcels each requiring access drives should share a common, centrally located access drive.
- f. Parking lots facing public streets, major pedestrian walkways, or plazas and open space shall incorporate a minimum eight foot wide landscaped setback with parking lot screen along the entire length of visual exposure (Graphic 5).
- g. Surface parking lots shall have a minimum of one shade tree for every six spaces in landscaped islands and / or on the perimeter of the lot. A minimum of ten square feet of landscaped surface area must be provided for each parking space, incorporated into islands and / or perimeter landscape treatment, with (1) 5-gallon bush per 10 square feet of area.
- h. Above grade parking structures shall be set back from streets a minimum of ten feet, except where desirable ground floor uses such as restaurants, retail, and commercial facilities are incorporated into the first floor of the structure facing the street. In this case, the setback is not required and appropriate build-to line designation zones shall apply. The ten foot required setback shall have sufficient landscaping to screen the structure at the pedestrian level, thus providing a visual amenity.
- i. Bumpers or tire stops shall be provided in all parking stalls heading into a sidewalk, planting area, street, or alley. They shall be installed at a minimum of two feet from the beginning of the sidewalk, planting area, street, or alley in order to effectively protect walkways, landscaping and signage.
- j. Exterior walls of all parking structures shall be designed as part of the architectural form of the main building or surrounding buildings. The intent is to minimize a garage appearance through designs that conform to the accompanying development's general architectural features.





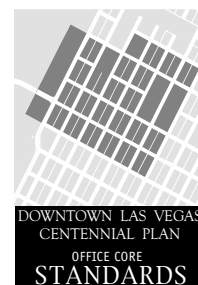
- k. Bus drop-off areas shall be provided in close proximity to the entrances of newly constructed hotels. Parking for buses shall be accommodated off-site or in a remote, non-obtrusive location so as not to interfere with normal traffic flow or pedestrian activity to and from the hotel site.
- l. Parking, loading and traffic development standards other than those described above shall be as set forth in LVMC 19.06.060.



## OC3. OFFICE CORE: STREETSCAPE DESIGN STANDARDS

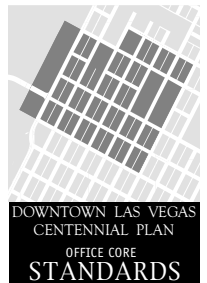
**1** All of the following streetscape design standards for the Office Core District also pertain to Downtown Las Vegas as a whole:

- a. North-south streets shall be designed thematically with palm trees as the primary landscape element (25 feet or greater in height spaced at 30 feet preferred, however, 35 feet maximum spacing). Shade trees, in single or double rows, may be provided alternately between palm trees to provide shade and pedestrian comfort (Graphic 7).
- b. East-west streets shall be designed thematically with shade trees as the primary landscape element (36 inch box trees spaced at 15 to 20 feet preferred to provide an immediate shade canopy, 24 inch box trees at 30 feet maximum spacing required). When shade trees are provided in lieu of an arcade or other architectural shade device, a double row of 36 inch box trees placed 15 feet on center is encouraged (Graphic 8).
- c. Supporting landscape and streetscape improvements shall be incorporated into the streetscape design, including decorative paving, lighting, and other landscape elements such as freestanding planters, planter boxes and hanging baskets. Paving must be relatively flat, slip resistant, and of a design that does not create a tripping hazard but adds visual interest and unique imagery. The surface of the sidewalk should appear as a consistent pattern. At the corners, the sidewalk must ramp down to provide for handicapped access.
- d. Street lights and decorative pedestrian lights shall be incorporated into any future streetscape improvement. Overhead lights must provide the ambient light necessary for safety and traffic operations, and it must meet City of Las Vegas Department of Public Works standards. (See specification in Item “e” below.) Pedestrian lights should be placed in a clear geometric pattern with regular spacing to add a sense of order to the street. The design of the lights and the pattern for their placement are part of the overall design for Downtown Las Vegas. The lights reinforce the hierarchical street system. Therefore, lights associated with individual development projects are prohibited from being located in the right-of-way.
- e. All new streetscapes, sidewalks, and streetscape elements shall conform to exactly match those installed within the Fourth Street Corridor and Las Vegas Boulevard Beautification Projects during 1996 to 1999. (Exact specifications, shop drawings, and standard suppliers can be obtained from the City of Las Vegas Engineering Design Superintendent, Department of Public Works, 229-6272.)

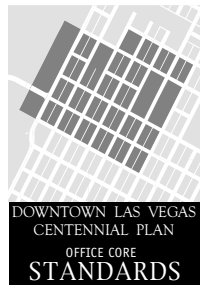


## OC4. OFFICE CORE: ARCHITECTURAL DESIGN STANDARDS

- 1** All of the following architectural design standards for the Office Core District, except the first two standards, also pertain to Downtown Las Vegas as a whole:
- a. All new or renovated buildings and structures shall provide ground floor retail (or other pedestrian supported activities) along seventy percent (70%) of the building streetwall, for a minimum depth of twenty five feet (25') from the outside of the streetwall.
  - b. Seventy percent (70%) of walls at the ground level on primary pedestrian routes and streets shall be composed of transparent glazing. Reflective glass at the pedestrian level is strictly prohibited. Glass on the second floor and above shall not be greater than twenty two percent (22%) reflectivity so as to not reflect light and solar heat on other buildings, streets and sidewalks.
  - c. Seventy percent (70%) of the building streetwall shall align along the edge of the sidewalk. If adjacent buildings are setback, the seventy percent (70%) of the building streetwall shall align within the median setback range of existing buildings likely to remain within the same block and street frontage. Setback requirements may be waived by staff or City Council for buildings that substantially enhance the pedestrian environment through the provision of other pedestrian or civic amenities such as: plazas, courtyards, landscape features, public entries, shade devices, cafes, etc.
  - d. Roofs and rooftops shall be designed with consideration to views from above. No highly reflective materials or contrasting colors shall be used. All mechanical equipment shall be screened from street level and surrounding building views. All obtrusive features shall be minimized.
  - e. An articulated roof line and / or an articulated cornice shall be designed as a major feature at or near the top of all streetwalls and walls visible from any right-of-way. Buildings with flat roofs shall incorporate a parapet wall around the entire perimeter of the building to shield the roof from street view and hide mechanical equipment, etc.
  - f. Extended, blank, expressionless walls at the street level shall be prohibited. The use of expression lines and expression zones (that is, areas and features of the facade that incorporate changes in material, color, and / or relief) shall be required in the pedestrian zone to create visually interesting facades (Graphic 9).
  - g. The use of arcades, awnings, canopies, and other shade devices is strongly encouraged.
  - h. The incorporation of misting systems at the pedestrian level is also strongly encouraged.
  - i. The main entry of the building from the street shall be appropriately articulated in the architectural design of the building. This shall be accomplished through change in materials, colors and / or the amount of detailing around the entry; having the entry slightly recessed or protruding from the primary building line; and / or through the use of canopies or awnings, etc.



- j. Exterior materials of all buildings shall contribute to a cohesive physical environment, convey a sense of permanence and belonging, and preferably be of natural, native materials.
- k. Setbacks on streetwalls facing public streets and plazas shall be required (to reduce the effects of wind traveling downward along the building face) to add visual interest and promote light penetration at the street level. Setbacks are required at a minimum of four stories, 11 stories, and 18 stories in building height. Each setback must be at least five feet in width (Graphic 10).
- l. Service areas (including storage, special equipment, maintenance, and loading areas) shall be screened with landscaping and / or architectural elements from pedestrian or street view. Architectural screening shall be constructed of the same materials and finishes as the primary building.
- m. All service and loading areas shall be positioned so service vehicles will not disrupt traffic flow to or from the site or within parking lots. No service, storage, maintenance or loading area may extend into a setback area.



## OC5. OFFICE CORE: SIGNAGE DESIGN STANDARDS

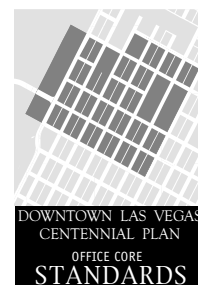
- 1 Within the Office Core District, the design, installation, and maintenance of all signs, shall be in full conformance with Title 19.14, Sign Standards, as revised and amended, unless subsequent more restrictive standards are promulgated and amended hereto.

**1** Landscape design standards shall be as set forth in LVMC 19.06.060.

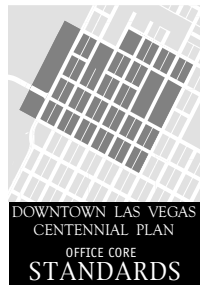
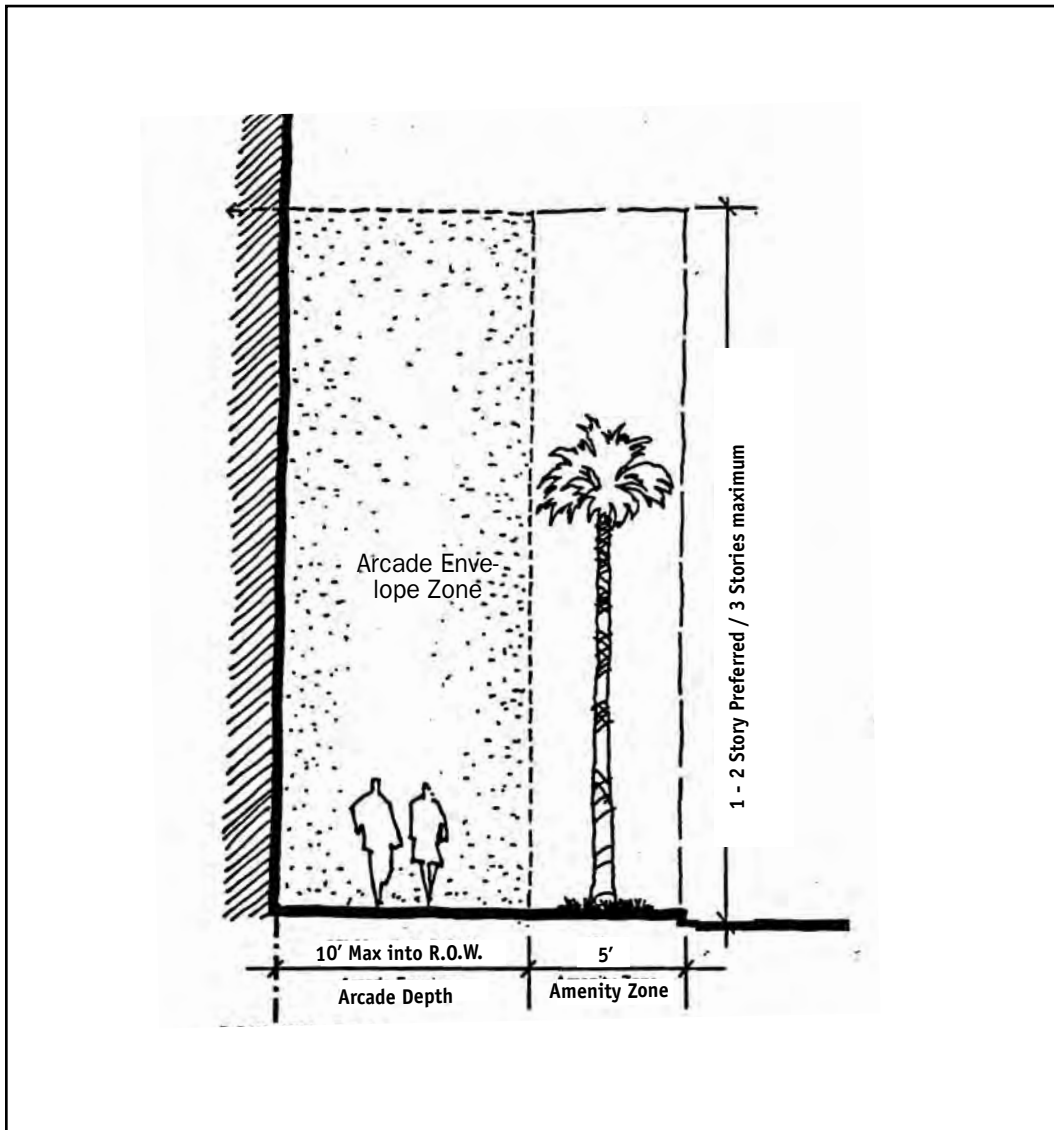
## OC7. OFFICE CORE: ARCADE DESIGN STANDARDS

**1** These are the minimum arcade design standards (Graphic 11) for the Office Core District when arcades are provided:

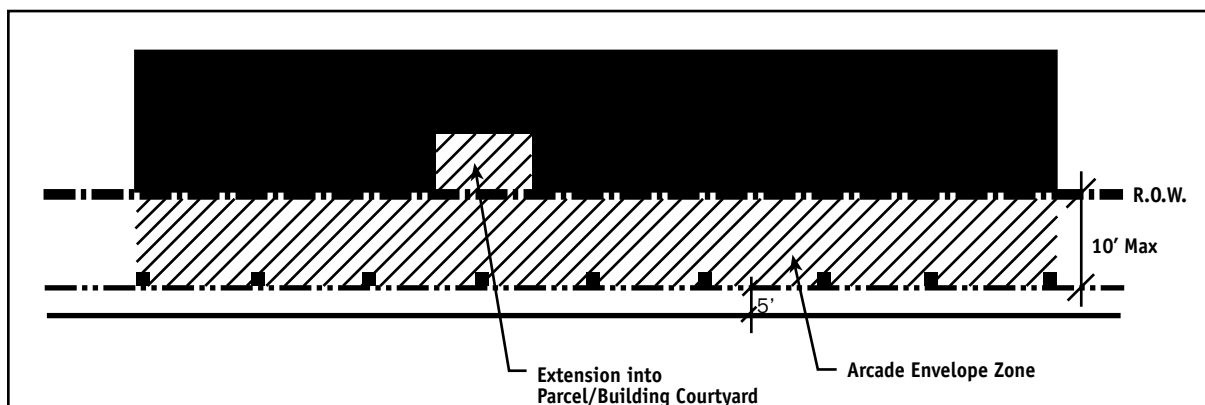
- a. All arcades must meet the fundamental goal of providing a sun screen, shade, or shelter for the pedestrian on the sidewalk. This goal must be accomplished to accommodate the angles of the sun from 11:00 a.m. to 3:00 p.m., May 15 to September 15 of each year.
- b. Signage advertising businesses and their addresses may occur on the arcade either under the protective cover or on the face of the arcade fronting the street. No sign occupying space beneath the protective cover may exceed eight square feet in area. No sign applied to the face of the arcade fronting a street may exceed 24 square feet in area. Neon and external illumination of signage is acceptable. Large, internally illuminated, back-lit panels are prohibited. Free-standing signs are prohibited.
- c. Design creativity and design excellence are encouraged in the execution of arcades. As part of the commercial heritage of Las Vegas, innovative responses to the design of arcades will contribute to an enduring image, character, and culture of Las Vegas. Adherence to the guiding principles of streetscape continuity, district image, pedestrian comfort, and streetwall uniformity are very important.



TYPICAL SECTION

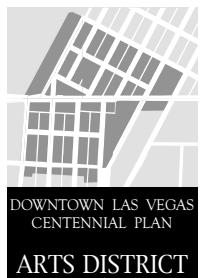


TYPICAL PLAN VIEW

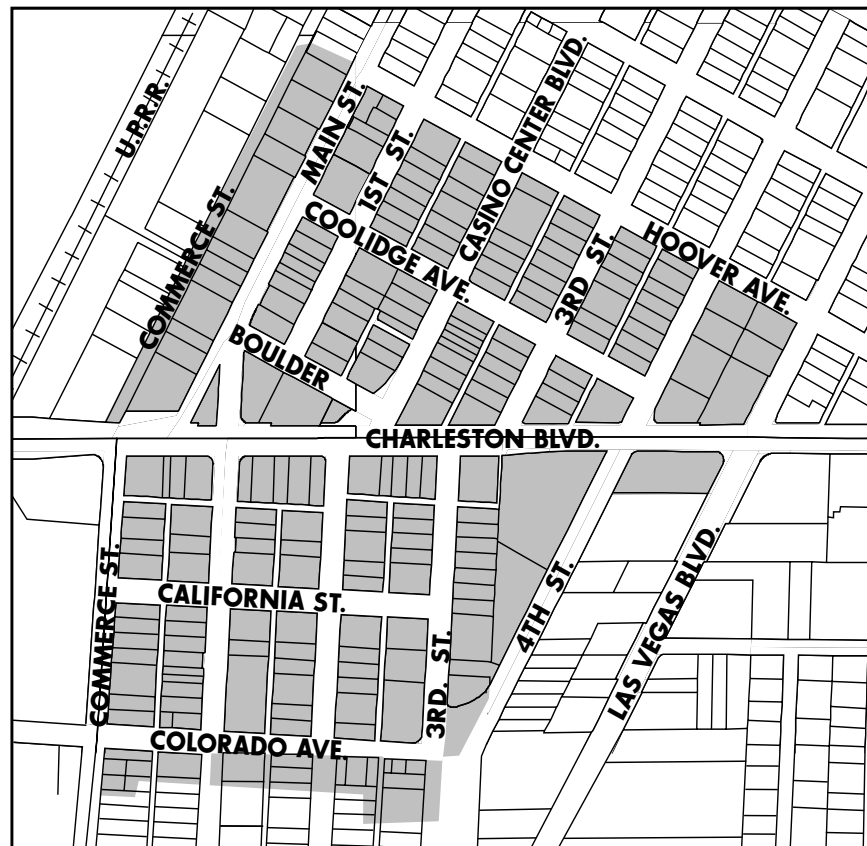


## AD. ARTS DISTRICT: DESIGN STANDARDS

**1** The following Arts District Design Standards apply only to the Arts District. Those standards that are more restrictive or vary from the Downtown Las Vegas design standards appear in bold, see following notes. The purpose and intent of the Arts district is to establish an atmosphere that fosters creativity and freedom of expression of multiple tastes and cultures. A vibrant and dynamic arts district is by nature distinct from surrounding neighborhoods. Outdoor uses of public and private space in the Arts District are strongly encouraged. The purpose of these standards is to foster the expression of an aestheticized lifestyle and as a result should be visually distinguished from the rest of downtown Las Vegas.



MAP 15: ARTS DISTRICT: DISTRICT BOUNDARY



**1** These are the site planning standards for the Arts District:

**a. LAND USES: Only the following uses are permitted within the Arts District:**

**1. Residential and Lodging Uses**

- Condominium
- Home Occupation
- Hotel or Motel
- Multifamily
- Mixed-Use
- Single Family - Attached
- Townhouse

**2. Institutional and Community Service Uses**

- Child Care Center
- College, or University (Special Use Permit required)
- Government Facility
- Library, Art Gallery or Museum – Public
- Music Conservatories, Dance, Culinary and Theatre Arts Schools and Art Instruction
- Public or Private School, Primary (Special Use Permit required)
- Public or Private School, Secondary (Special Use Permit required)

**3. Office and Professional Uses**

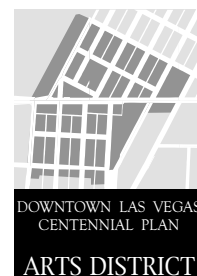
- Financial Institution without Drive-Through
- Hypnotherapy
- Medical Office
- Office, Other Than Listed

**4. Recreation, Entertainment and Amusement Uses**

- Billiard Parlor or Pool Hall (Special Use Permit required)
- Commercial Amusement/Recreation – Indoor/Outdoor
- Health Club
- Night Club (Special Use Permit required)
- Private Club, Lodge or Fraternal Organization
- Public Park or Playground (SUP required)

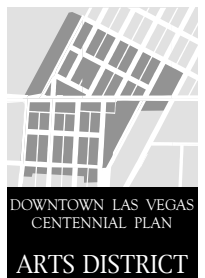
**5. Retail and Personal Services**

- Antique/Collectible Store
- Astrologer, Hypnotist, or Psychic Art and Science (Special Use Permit required)
- Catering Service
- Copy Center
- Dry Cleaners





- General Personal Service
  - General Retail Store, Other Than Listed, 3500 sq. ft. or More
  - General Retail Store, Other Than Listed, Less Than 3500 sq. ft.
  - Hotel Lounge Bar (Special Use Permit required)
  - Jewelry Store, Class III (Special Use Permit required)
  - Jewelry Store, New
  - Laundry, Self-Service
  - Liquor Establishment – Tavern (Special Use Permit required)
  - Museum or Art Gallery – Private
  - Restaurant with Service Bar (Special Use Permit required)
  - Restaurant, 2000 sq. ft. or more, w/o Drive-Through
  - Restaurant, Less than 2000 sq. ft., w/o Drive-Through
  - Supper Club (Special Use Permit required)
  - Tattoo Parlor/Body Piercing Studio (Special Use Permit required)
  - Used Books
  - Used/Vintage Clothing
  - Used CD's/Records/Videos
  - Wedding Chapel
  - Wedding Chapel, with Drive-Through (Special Use Permit required)
6. Commercial and Business Services
- Desktop Publishing
  - Electrical, Watch, Clock, Jewelry and Similar Repair
  - Off-Premise Advertising Sign (Special Use Permit required). In addition to the complying with all criteria listed in Title 19, Off-Premise Advertising Signs shall be directly mounted to and flush with the wall of a structure, and shall not be freestanding, mounted on top of a roof, or project past the walls or roof of a building.
7. Industrial and Manufacturing
- Light Assembly and Fabrication (Conditional Use Permit required)
  - Printing and Publishing (Special Use Permit required)
8. Wholesale, Distribution and Storage
- Wholesale Showroom Facility (Special Use Permit required)
9. Utilities, Communications and Transportation
- Monorail (Special Use Permit required)
  - Mounted Antenna of 15' or less
  - Parking Structure (Special Use Permit required)
  - Recording Studio
- Uses which require a Conditional Use Permit shall adhere to the process and conditions stipulated in Title 19.04.040. Uses which require a Special Use Permit shall adhere to the process and minimum requirements stipulated in Title 19.04.050.

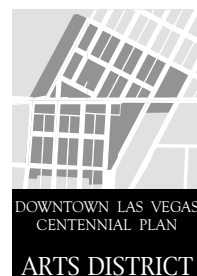


Passive uses such as offices; financial institutions; government facilities, medical, law and other offices; copy centers, desktop publishing and light assembly facilities are limited to no more than 25% of the building facade.

**2** LOT COVERAGE. Within the Arts District, lot coverage may range up to a maximum of 100 percent. No minimum lot coverage has been established.

## AD2. ARTS DISTRICT: TRANSPORTATION AND PARKING STANDARDS

- 1** All of the following transportation and parking standards for the Arts District also pertain to Downtown Las Vegas as a whole:
- a. All off-street surface parking lots shall be paved and include access aisles and drive-ways. Surface treatments including paving and striping shall be maintained in good condition at all times.
  - b. Whenever a driveway is abandoned, the owner shall remove all driveway pavement and approach, re-install the curb and gutter.
  - c. Parking areas shall be separated from buildings through surface treatment variations and/or grade separation.
  - d. Wherever possible, adjacent parcels each requiring access drives shall share a common, centrally located access drive.
  - e. **Surface parking lots shall be located behind buildings at the interior of the block wherever possible. Access to parking lots from existing alleys is preferred, or from east-west streets where no alley access is available. Access to parking lots from north-south streets and Charleston Boulevard is discouraged in order to maintain a continuous building line and prevent interruptions in the pedestrian network.**
  - f. Surface parking lots shall provide shading elements either natural or artificial.
  - g. Above grade parking structures may be set back from streets a minimum of ten feet, where desirable ground floor uses such as restaurants, retail, and commercial facilities are incorporated into the first floor of the structure facing the street. Building design shall provide for visual continuity between the pedestrian experience and the interior use.
  - h. Bumpers or tire stops shall be provided in all parking stalls heading into a sidewalk, planting area, street, or alley. They shall be installed at a minimum of two feet from the beginning of the sidewalk, planting area, street, or alley in order to effectively protect walkways, landscaping, and signage.

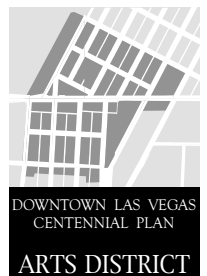


- i. All new developments shall provide and install standard Fourth Street style fixtures in place of existing fixtures. Exact specifications, shop drawings, and standard suppliers can be obtained from the City of Las Vegas Engineering Design Superintendent, Department of Public Works.
- j. Parking structures shall be considered primary design expression opportunities and contribute to the aesthetic character of the district through creative design solutions for the structure, facades and screening elements.

### AD3. ARTS DISTRICT: STREETSCAPE DESIGN STANDARDS

**1** All of the following streetscape design standards for the Arts District also pertain to Downtown Las Vegas as a whole:

- a. North-south streets shall be designed thematically with palm trees as the primary landscape element (25 feet or greater in height spaced at 30 feet on center preferred, however, 35 feet maximum spacing). Shade trees in single or double rows, may be provided alternately between palm trees to provide shade and pedestrian comfort (Graphic 7).
- b. East-west streets shall be designed thematically with shade trees as the primary landscape element (36-inch box trees spaced at 15 to 20 feet preferred to provide an immediate shade canopy, 24-inch box trees at 30 feet maximum spacing required). When shade trees are provided in lieu of an arcade or other architectural shade device, a double row of 36-inch box trees placed 15 feet on center is encouraged (Graphic 8).
- c. Supporting landscape and streetscape improvements shall be incorporated into the streetscape design, including decorative paving, lighting, and other landscape elements such as freestanding planters, planter boxes and hanging baskets. Public artwork is encouraged as an element of the streetscape design in the Arts District, provided it does not encroach pedestrian or vehicular travel lanes, or pose any visibility or safety hazards. Paving must be relatively flat, slip-resistant, and of a design that does not create a tripping hazard but adds visual interest and unique imagery. The surface of the sidewalk should appear as a consistent pattern. At the corners, the sidewalk must ramp down to provide for handicapped access.
- d. Street lights and decorative pedestrian lights shall be incorporated into any future streetscape improvement. Overhead lights must provide the ambient light necessary for safety and traffic operations, and it must meet City of Las Vegas Department of Public Works standards. (See specification in Item “e” below.) Pedestrian lights should be placed in a clear geometric pattern with regular spacing to add a sense



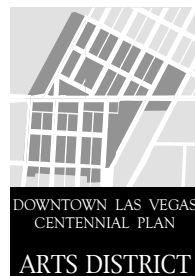
of order to the street. The design of the lights and the pattern for their placement are a part of the system. Therefore, lights associated with individual development projects are prohibited from being located in the right-of-way.

- e. All new streetscapes, sidewalks, and streetscape elements shall conform to exactly match those installed within the Fourth Street Corridor, Las Vegas Boulevard, and the Lewis Avenue Corridor beautification Projects during 1995 to 2002. (Exact specifications, shop drawings, and standard suppliers can be obtained from the City of Las Vegas Engineering Design Superintendent, Department of Public Works, 229-6272.)

#### AD4. ARTS DISTRICT: ARCHITECTURAL DESIGN STANDARDS

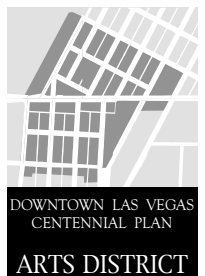
**1** The following architectural design standards pertain to properties within the Arts District. Those standards that are more restrictive or vary from the Downtown Las Vegas design standards appear in bold below.

- a. **Facade Configuration:** **The goal of the arts District is to encourage building design that contributes to the quality of life in an active, aesthetically considered manner. Street level design should enliven the street with visual continuity of activity from interior uses to the pedestrian experience. Extended, solid walls are discouraged at the street level. Wherever solid walls are present, surface treatment that contributes to the aesthetic character of the facade is encouraged to provide visual interest. This may include but is not limited to color, murals, surface articulation, texture, and graphics.**
- b. Shading/Climate Control: The use of arcades, awnings, canopies, and other shade devices is strongly encouraged. The incorporation of misting systems at the pedestrian level is also strongly encouraged to complement the objectives of this plan.
- c. Exterior Materials: **Exterior materials of all buildings shall contribute to the artistic character and quality of life of the district.**
- d. Screening Requirements: Service areas (including storage, special equipment, maintenance, trash dumpsters and loading areas) shall be screened from pedestrian or street view with elements that contribute to the aesthetic expression of the building.
- e. Service and Loading Areas: All service and loading areas shall be positioned so service vehicles will not disrupt traffic flow to or from the site or within parking lots. No service, storage, maintenance or loading area may extend into a setback area.
- f. Meters: Utility meters shall be screened from view at street level.



**1** These are the standards for signage in the Arts District:

- a. General Requirements. In order to encourage a diverse and visually interesting environment, signage in the Arts District is exempt from the automatic application of size, type and area limitations. Signage shall reflect the special character of the arts District. Prosaic signs are discouraged. However, the exemption does not prohibit City staff, the Planning Commission, and the City Council from imposing limitations on the approval of such signage. Construction, location, installation, maintenance and issuance of certificates for signage shall be in accordance with Title 19.14.
- b. Review Process. Signage for Arts District properties shall be reviewed and approved in the following manner:
  - 1) For development that is subject to the Site Development Plan Review process as described in Title 19.18, signage shall be reviewed and approved as part of the development plan for the property. Signage shall be reviewed for conformance with the criteria listed in Section AD5(1-c).
  - 2) For all other applications, signage shall be reviewed and approved by a representative of each of the following City departments: Planning and Development, Office of Business Development, and Leisure Services. Signage shall be reviewed for conformance with the criteria listed in Section AD5(1-c).
- c. Review Criteria. Signage in the Arts District shall be reviewed and approved subject to the following criteria:
  - 1) The signage shall reflect the arts-related nature of the district by incorporating artistic details, sculptural elements, three-dimensional features, ornate designs, or other similar expressive elements;
  - 2) The signage may be internally or externally illuminated, provided the illumination does not create unnecessary glare or pose impacts to residential units or abutting properties.
- d. Non-Commercial Messages. Murals or other artistic treatments that do not contain a commercial message are not considered signage.



## AD6. ARTS DISTRICT: LANDSCAPE DESIGN STANDARDS

**1** Within the Arts District, landscaping and planting materials shall be installed per, and be in full conformance with Title 19.12, Landscape and Buffer Standards, and with the landscape requirements contained in Title 19.08.45, Commercial District Development Standards. Where specific landscape standards or elements are included with the District Plans contained herein, they shall supercede these and other existing standards.

## AD7. ARTS DISTRICT: TRANSIT CORRIDOR DESIGN STANDARDS

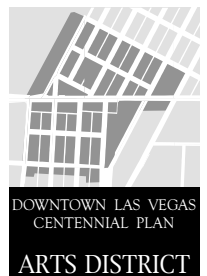
**1** The following design regulations have been developed to address specific conditions relative to Arts District properties along the route of the Las Vegas Monorail and within the vicinity of the monorail station. Refer to Sections AD1 – AD6 for any standards not addressed herein.

- a. **Applicability:** Properties that directly abut the Las Vegas Monorail right-of-way are subject to the Transit Corridor Design Standards. Additional standards that apply to properties in the vicinity of the monorail station are identified below.
- b. **Uses:** The uses specified in Section AD1(1) of this document are permitted in the Transit Corridor, with the exception of the following:
  - Single Family – Attached
  - Townhouse

Due to issues relative to noise and privacy, permitted residential uses shall not be allowed on the first or second story of buildings.

- c. **Transportation and Parking Standards:**
  - 1) **Surface parking lots:** Surface parking lots are permitted with approval of a Special Use Permit (SUP).
  - 2) **Parking Structures:** Parking structures are encouraged within 600 feet of the Guideway station, subject to the design standards contained in Section AD2. Parking structures should be secondary to commercial and residential activity.
- d. **Architectural Design Standards:**

**Roofs and Parapets:** Roofs and rooftops shall be designed with consideration for views from the Guideway. Rooftop artwork and architectural embellishments are encouraged.



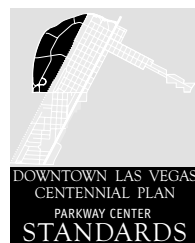
## PART 2: PARKWAY CENTER DEVELOPMENT STANDARDS

APPROVED BY CITY COUNCIL ON JANUARY 2, 2002, LESA CODER, DIRECTOR OF BUSINESS DEVELOPMENT

### INTRODUCTION

#### EXHIBIT ONE: PARKWAY CENTER PRE-CONSTRUCTION, 2001

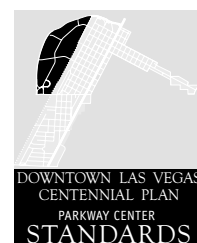
The Union Pacific Railroad founded Las Vegas on May 5, 1905, as a commercial real estate venture. The railroad's realtor laid out a grid of 40 dusty blocks just east of the Mainline at a watering hole between Los Angeles and Salt Lake City.



The switching yards began to fill out to the west. The City of Las Vegas was incorporated in 1911, and began to grow along Fremont Street to the east, and then spread to new subdivisions to the east and south. It was not until 4 or 5 decades later that the west side began to grow beyond the railyards.



Highways were built to bring people in and out of downtown, and up and down the Strip. Eventually a hole was left in the core of our city, but a hole at the desert crossroads of the Southwest. Parkway Center turns out to be the first great urban real estate deal of the 21st Century—200 clean acres in the center of the fastest growing city in the nation.

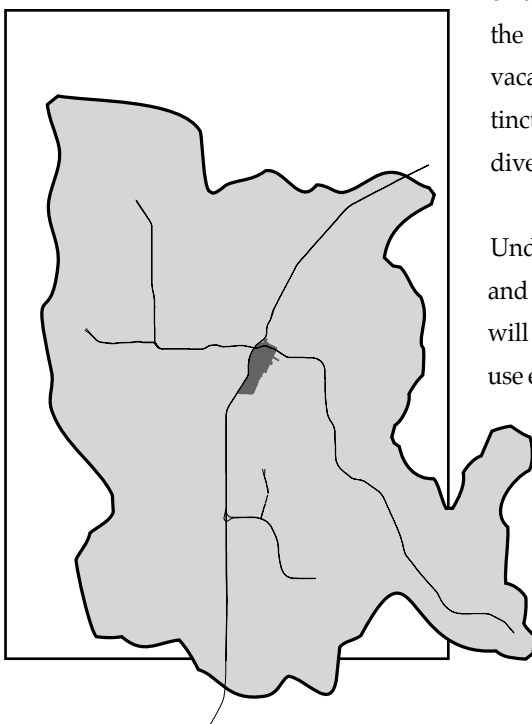


Parkway Center encompasses the entire former Union Pacific railyards, which in part, is already home to the Clark County Government Center. The district is bound by Interstate 15 and the “Spaghetti Bowl” on the east, U.S. 95 / Interstate 515 on the north, the

Union-Pacific Railroad tracks right-of-way on the east, and Charleston Boulevard on the south. The remainder of the site is vacant and prepared for intense and distinctive redevelopments that will further diversify our economy.

Under the direction of Mayor Goodman and this City Council, Parkway Center will become an urbane, walkable, mixed-use environment of national prominence.

Parkway Center will become the Desert Crossroads of Las Vegas.





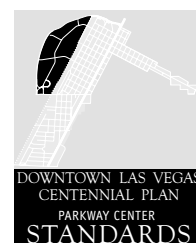
## EXHIBIT TWO: PARKWAY CENTER AFTER PHASE ONE AND PHASE TWO DEVELOPMENT, 2011

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With the help of the railroad, today the City, and several prominent national and international developers, is preparing to rebuild those train yards into one of the finest, new urban centers in the nation. So now, the fastest growing city will also be known as the leader in urban redevelopment.

New and exciting land uses are currently in development or discussions (see attached colored Map for locations), such as:

1. University medical / research campus with student housing
2. International trade center, with major wholesale and retail showrooms for furniture and other durable goods
3. Regional shopping center, and other pedestrian-friendly retail experiences
4. High-tech offices and incubators, computer server farms, and internet backbone facilities
5. Performing arts center, with other cultural and artistic venues
6. Other Class A office towers
7. High-density mid-rise to high-rise market-rate condominiums
8. Convention, arena, and entertainment / production facilities
9. Non-gaming hotels and perhaps limited casino/hotels
10. Possibly a professional sports stadium



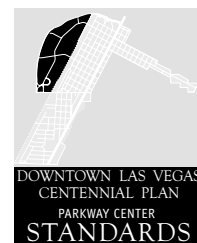
Pedestrian linkages to Fremont Street Experience, the Office Core District, and the Arts District will be very important to the City. Parkway Center must function as a natural extension of the existing downtown to enhance the businesses that already exist in the downtown core districts.

A tremendous opportunity is now emerging as we work with the Regional Transportation Commission to develop an intermodal transportation center, a "Downtown Hub," that will link together the RTC Monorail, the CAT bus system, private cross-country bus systems, and future Amtrak high-speed trains and the proposed future Transrapid super-speed train. This transit and pedestrian center will also help to link the Office Core to the City owned 61-acre development parcel, by moving people up and over the existing train right-of-way, and by providing substantial parking and retail facilities.

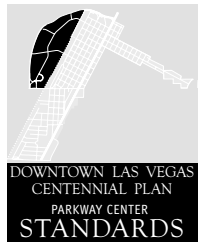
In early 1999, the former Union Pacific railyards were rezoned to Planned Development (PD) under Title 19A of the City codes, from its former Industrial Manufacturing (M)

zone (via Z-100-98). At that time, rudimentary design standards were incorporated into the PD zoning, as were lists of allowable and disallowable uses. In July of 2000, the Downtown Centennial Plan was adopted that contains development standards for the entire downtown area including Parkway Center. That PD zoning, the City's 2020 Master Plan, and the Downtown Centennial Plan were revised recently to allow non-restricted gaming on certain parcels meeting distance separation requirements.

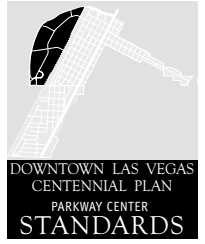
Today, staff is proposing to develop more intense design and development standards for Parkway Center honed to the emerging opportunities that are now presenting themselves. By carefully crafting development standards before construction begins on Parkway Center, the City Council and staff will be able to protect the future interest of our Citizens. We will be able to ensure that only quality, top-notch development occurs on the several Parkway Center parcels, and that all development is linked to transportation and pedestrian systems. These development standards could be incorporated as an amendment to the Downtown Centennial Plan.













## 1. GENERAL DEVELOPMENT STANDARDS

1.1. Permitted Land Uses: Within all of Parkway Center, unless otherwise specifically prohibited herein, any of the following business, government, public or private land uses shall be permitted on the property, and shall be allowed in any vertically or horizontally mixed-use configuration within the same building or group of buildings, parcel or group of parcels.

1.1.1. Commercial Office, Class A, including, without limitation:

- 1.1.1.1. Professional and General
- 1.1.1.2. High-Technology
- 1.1.1.3. Service
- 1.1.1.4. Research and Development
- 1.1.1.5. Major or Minor Medical

1.1.2. Flexible Office / Work Space, Class B, including, without limitation:

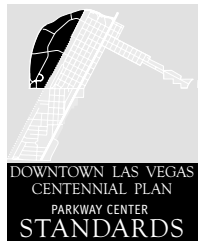
- 1.1.2.1. Internet and high-technology incubators
- 1.1.2.2. Computer server farms
- 1.1.2.3. Internet backbone facilities

1.1.3. Government / Public Office, including, without limitation:

- 1.1.3.1. United States
- 1.1.3.2. State of Nevada
- 1.1.3.3. Regional Bodies and related
- 1.1.3.4. Clark County
- 1.1.3.5. City of Las Vegas
- 1.1.3.6. United States Post Office

1.1.4. Commercial/Retail, including, without limitation:

- 1.1.4.1. General Retail
- 1.1.4.2. Restaurant/Delicatessen/Coffee Shop/Bakery
- 1.1.4.3. Health Club
- 1.1.4.4. Bank / Credit Union / Savings and Loan / Trust
- 1.1.4.5. Children's Day Care
- 1.1.4.6. Convenience Retail
- 1.1.4.7. Offices with frequent public access requirements
- 1.1.4.8. Trade Center / Wholesale and Retail Showrooms
- 1.1.4.9. Exhibition Space



1.1.4.10. Transportation Services, including, without limitation:

- 1.1.4.10.1. Transportation Center
- 1.1.4.10.2. Heliport
- 1.1.4.10.3. Monorail
- 1.1.4.10.4. Gasoline Sales Facilities

1.1.4.11. Parking Facilities, including multi-level parking structures

1.1.4.12. Entertainment/Retail

1.1.4.13. Convention / Conferencing Facilities / Temporary Exhibitions

1.1.4.14. Non-Profit / Institutional, including:

- 1.1.4.14.1. Museums
- 1.1.4.14.2. Performing arts centers
- 1.1.4.14.3. Musical theaters, etc.

1.1.4.15. High Density Residential, market rate, including, without limitation:

1.1.4.15.1. Mid-Rise Housing, 4-7 stories, including:

- 1.1.4.15.1.1. Courtyard / streetwall apartments or condominiums
- 1.1.4.15.1.2. Loft-style apartments or condominiums

1.1.4.15.2. High-Rise Housing, 8 stories and up, apartments or condominiums

1.1.4.16. Non-Gaming Hotels

1.1.4.17. Casino / Hotels, but only within the Gaming Enterprise Overlay District, as amended

1.1.4.18. Non-restricted Gaming, but only with the Gaming Enterprise Overlay District, as amended

1.1.4.19. Existing Interim Billboard Signs, to be removed or incorporated with adjacent development

1.2. Prohibited Land Uses: Within all of Parkway Center, unless otherwise specifically permitted herein, the following land uses shall be prohibited.

1.2.1. Trailer Parks and RV Camps

1.2.2. Transient Sales Lots

1.2.3. Rental Storage Businesses, with outdoor storage

1.2.4. Contractors Plant, Shop and Storage Yard

1.2.5. Construction Materials and Supply Yards

1.2.6. Auto Repair Garages

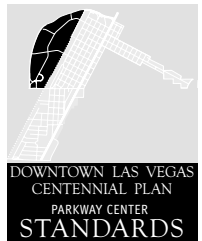
1.2.7. Asphalt and Concrete Batch Plants

1.2.8. Environmentally Hazardous Materials

1.2.9. Salvage or Reclamation Yards

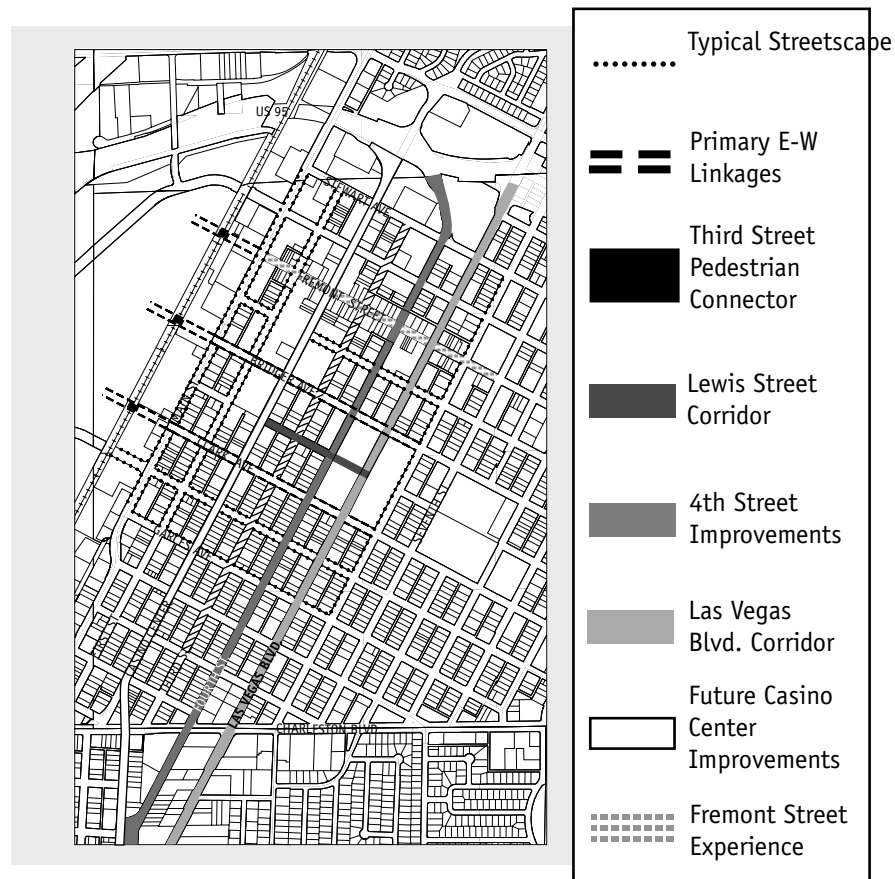
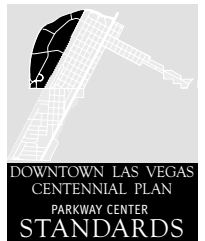
1.2.10. Landfill

1.2.11. Outside Storage



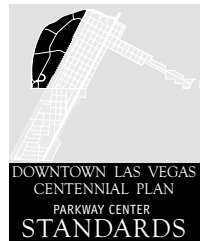
- 1.2.12. Trucking Company
- 1.2.13. Heavy Manufacturing
- 1.2.14. Towing and Impound Yards
- 1.2.15. Cold Storage Plants
- 1.2.16. Wholesale Distribution Centers
- 1.2.17. Sexually Oriented Businesses
- 1.2.18. Animal Care Facilities
- 1.2.19. Any other use that would otherwise be permitted as-of-right in Manufacturing (M) or Commercial Manufacturing (CM) zoning districts

1.3. Pedestrian Connectivity and Accommodation: Special attention shall be given to pedestrian connectivity across and through Parkway Center, and between Parkway Center and the existing downtown core districts to the east. The creation of a seamless and highly accommodating pedestrian network that mitigates the current obstacles that the railroad right-of-way presents must be developed. The Downtown Centennial Plan calls for at least three (3) primary pedestrian linkages up and over the train tracks at or near Fremont Street, Bridger Avenue, and Clark Avenue. The Bridger and/or Clark pedestrian routes could potentially be incorporated into an intermodal transportation center that could easily link pedestrians from Parkway Center and the downtown core with the monorail and buses, and other related uses.



1.3.1. Sidewalks: All sidewalks within Parkway Center shall at a minimum, especially as regards minimum width and streetscape amenities, comply with the minimum standards of the Downtown Centennial Plan. Special attention shall be given to the differentiation of North-South and East-West streetscape typologies as found within the Plan. Staff or the PC-ARC may determine that greater sidewalk width or a higher level of streetscape amenity shall be required for particular development proposals or in general at some future time that Parkway Center becomes highly pedestrianized.

1.3.2. Transportation Trail System: City staff shall create a Parkway Center Master Trails Plan for pedestrians and bicyclists, etc., as a part of these development standards. Such trail system, in part, shall fall within landscaped buffer areas adjacent to the interstate highway system and the train right-of-way. Consideration shall also be given to the future linkage of the Parkway Center trail system to the proposed city-wide transportation trail system, and shall comply with the adopted Transportation Trails Element of the City of Las Vegas 2020 Master Plan.



1.4. Viewsheds: The pedestrian level views through and across Parkway Center, and the views from taller buildings down upon and across shorter buildings, shall be carefully considered within development proposals and from adjacent buildings and approved development proposals, and shall be subject to the review of the PC-ARC. The following standards shall apply.

1.4.1. Pedestrian Level Views: Applicants for development proposals shall give careful consideration to creating views at the pedestrian level that are visually pleasing, uniquely urban, and providing great amenity. The pedestrian level may be at ground level or upon raised plazas and courtyards, etc. Visual connection at the pedestrian level shall be continuous within development proposals, and continuous between and harmonious with adjacent buildings and approved development proposals.

1.4.2. View from Taller Building Upon Shorter Buildings: Applicants for development proposals shall give careful consideration to views from taller buildings and approved development proposals containing taller buildings, down upon shorter buildings. The views from existing taller buildings or approved development proposals containing taller buildings shall be protected from looking down upon proposed shorter buildings or development proposals containing shorter buildings. Applicants shall carefully employ a relationship of site planning, height, massing, and bulk that screens taller buildings from viewing the roofs and roof-



mounted mechanical equipment, etc., of proposed shorter buildings.

1.5. Building Height: Within Parkway Center, the following minimum and maximum building heights, relationships between the heights of buildings and development proposals, and definitions shall apply.

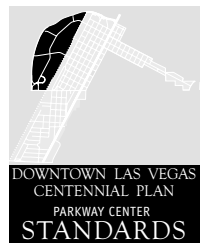
1.5.1. Minimum Building Height: All office and residential buildings shall be required to be at least four (4) stories in height. All other buildings and development proposals shall conform to a minimum building height of two (2) stories, and/or have a minimum parapet height of at least 35 feet in height.

1.5.2. Maximum Building Height: No maximum height restrictions shall be placed on any buildings or development proposals. All other heights shall be considered by the PC-ARC. The design of other buildings and development proposals shall be compatible with, and visually complimentary to, existing or proposed adjoining site developments, as regards to both height and bulk.

1.5.3. Congruity of Massing and Protection of Viewsheds: The relationship of the height of buildings and groups of buildings shall be considered, as regards the site planning, massing, and bulk of buildings, as well as the views from taller buildings down upon and across shorter buildings, and shall be subject to the review and approval of the PC-ARC.

1.5.4. Definitions of Height: Buildings, existing or proposed, or development proposals or portions thereof, shall be defined as “low-rise” if the buildings are one (1) or two (2) stories in height, or less than 35 feet in height. Buildings shall be defined as “mid-rise” if the buildings are three (3) to seven (7) stories in height, or greater than 35 feet in height but less than 75 feet in height. (Note: Within the City of Las Vegas, the Uniform Building Code has been amended to define high-rise buildings as those 55 feet and taller.) Buildings shall be defined as “high-rise” if the buildings are eight (8) stories or greater in height, or greater than 75 feet in height. The term “shorter buildings” shall be defined to mean low-rise and mid-rise buildings. The term “taller buildings” shall be defined to mean high-rise buildings.

1.6. Floor Area Ratio (“FAR”): Floor Area Ratio or FAR shall be defined to mean the ratio of gross floor area in square feet of a development proposal to gross parcel area in square feet of the parcel or parcels upon which the development will be built. A high density development is the goal of these development standards, but it is understood that realistic and financially feasible development proposals that rise from



market demand will influence density levels during initial phases of development. At full initial build-out it is the goal of these development standards to achieve a minimum FAR of 1.5 or greater. (Full initial build-out shall be defined to mean that time in which all Parkway Center parcels have viable and on-going first generation developments upon them.)

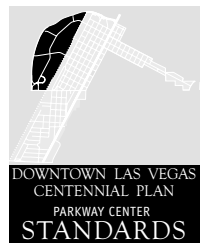
1.7. Lot Coverage: Lot Coverage shall be defined to mean the ratio of gross floor area in square feet of the maximum footprint or gross ground floor area of a development proposal to the total gross parcel area in square feet of the parcel or parcels upon which the development is to be built. Lot Coverage may range up to a maximum of 100 percent on certain parcels, so long as Usable Open Space requirements are met across an overall development proposal.

1.8. Usable Open Space: Usable Open Space shall be defined to mean the ratio of gross parcel area in square feet, which remains open and accessible to the public, to the total gross parcel area in square feet of the parcel or parcels upon which the development is to be built. At least 20 percent of a parcel or parcels across a total development proposal shall remain usable open space for the duration of survivability of any development. Open space shall be defined as usable if it is open and accessible to the public and contains intense landscaping or other pedestrian amenities and may include, but is not limited to, parks, pocket parks, plazas, garden spaces, outdoor café space, pergolas, arcades, and colonnades, etc., but shall not include public sidewalks as required as a condition of approval.

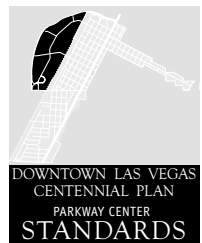
1.9. Temporary Structures: No temporary structures shall be permitted on any building site; provided, however, that attractive and well landscaped trailers, temporary buildings and similar shall be permitted only for a period of construction and pre-leasing purposes. Such structures shall be placed as inconspicuously as is practical, shall cause no inconvenience to owners or occupants or other building sites, and shall be removed not later than thirty (30) days after the date of substantial completion for beneficial occupancy of the building or group of buildings in connection with which the temporary structure was used, and in no case remain on site for a period of time greater than 24 months. The PC-ARC shall approve the location and type of temporary structures, and necessary extensions of time.

1.10. Application Process: Applicants with development proposals shall follow the Site Development Plan Review procedures within Title 19A of the City codes.

1.11. Design Review: There is hereby created the Parkway Center - Architecture Review



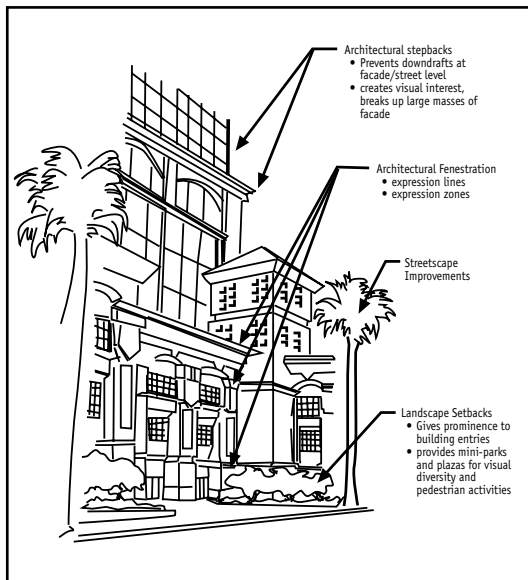
Committee (hereinafter “PC-ARC”). The PC-ARC shall review all Master Site Plans pursuant to the PD zoning and/or all Site Development Plan Review applications pursuant to Title 19A of the City codes, for conformity with the development standards contained herein, and shall exercise their discretion to recommend variances from the development standards for justifiable cause, and to recommend amendments to these standards as Parkway Center develops further and changing circumstances may warrant. The PC-ARC shall consist of five (5) members, who shall be appointed by the Mayor, and shall consist of two (2) Planning Commissioners, two (2) members of Planning and Development Department staff, and one (1) member of Office of Business Development / Redevelopment Agency staff. The PC-ARC shall meet prior to an item being heard at Planning Commission, and shall make their recommendation thereto via incorporation into normal staff reports. The PC-ARC may also meet with applicants between their petitions before Planning Commission and City Council, or during abeyance by either body, to further refine their plans, or to address specific issues raised by either body.



- 1.12. Applicability: These development standards shall apply to all new and proposed developments within Parkway Center. Where these development standards are in conflict with Title 19A of the City codes, the City of Las Vegas 2020 Master Plan, or the general standards of the Downtown Centennial Plan, then the more restrictive standard shall apply. When these development standards are silent on a particular design issue, than Title 19A of the City codes, the City of Las Vegas 2020 Master Plan, or the general standards of the Downtown Centennial Plan shall apply. These development standards shall be adopted as an amendment to the Downtown Centennial Plan, and shall form a part of the text and illustrations of the Plan as a new chapter similar to the Office Core District Design Standards.

## 2. SITE PLANNING AND PHYSICAL DESIGN

- 2.1. Seventy-five (75) percent of the building streetwall shall align with the street or primary pedestrian route and/or fall within the designated build-to-zones, and shall be parallel to the street. Setback requirements may be waived for buildings that enhance the pedestrian environment through the provision of other pedestrian or civic amenities such as plazas, courtyards, landscape features, public entries, shade devices, cafes, etc., per approval of the PC-ARC.
- 2.2. To encourage intensified pedestrian usage, and urban-life along streets and primary pedestrian routes, all primary building entries shall face the street or primary pedes-



trian route on the parcel.

2.3. Pitched roofs and flat or nearly flat rooftops shall be designed with consideration of silhouette views, and to views from above. No highly reflective materials or contrasting colors shall be used on horizontal surfaces.

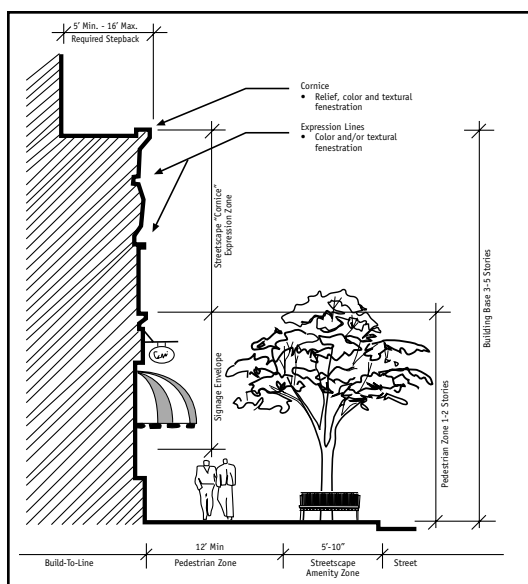
2.4. Extended, blank, expressionless walls at the street level shall be prohibited. Architectural detailing at the pedestrian level shall be required to create visually interesting facades.

2.5. Protection of ground floor or window glazing through the use of shaded arcades, awnings, canopies, and other shade or architectural devices is strongly encouraged.

2.6. The incorporation of misting systems at the pedestrian level is strongly encouraged.

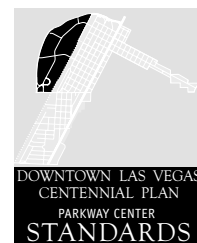
2.7. The main entry of any building from the street shall be appropriately articulated in the architectural design of the building.

2.8. Exterior materials of all buildings shall contribute to a cohesive physical environment, convey a sense of permanence, and preferably be of natural, regional materials, per the recommended color and materials palette, should the PC-ARC choose to create such palette.

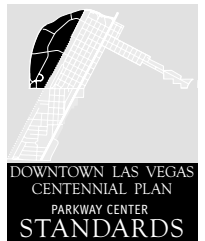


2.9. Setbacks and build-to zones along streets and primary pedestrian routes and beyond parcel lines may be established by the PC-ARC, as it deems necessary to maintain pedestrian continuity and connectivity within and between development proposals.

2.10. Service areas (including storage, special equipment, maintenance, and loading areas) shall be fully screened with landscaping and/or architectural elements from pedestrian, street view or neighboring property. Architectural screening shall be constructed of the same materials and finishes as the primary building and should be reinforced with landscape screening.



- 2.11. All service and loading areas shall be positioned so service vehicles shall not disrupt traffic flow to or from the site or within parking lots. No service, storage, maintenance, or loading area may extend into any required setback area, nor shall it be visible to public view from any street or primary pedestrian routes, and neighboring parcel ground floor spaces.
- 2.12. All mechanical equipment, including, but not limited to, roof-mounted equipment, shall be screened from view on all sides using materials and colors compatible with the building architecture. All mechanical equipment including, but not limited to, roof mounted equipment on buildings less than five stories in height shall also be screened from the top using materials and colors compatible with the building architecture and/or roofing materials.
- 2.13. Antennae, satellite dishes, and other communications devices shall be visually masked to the extent practicable and consistent with appropriate electromagnetic considerations.
- 2.14. All utility lines, without exception, shall be placed underground. No transformer, electric, gas, water, or other meter of any type or other apparatus shall be located on any power pole or hung on the outside of any building. Transformers shall be sited away from and also shall be screened from view from streets and primary pedestrian routes. Meters may be placed on or below the soil surface, and where so placed, shall be adequately screened from view.
- 2.15. Sustainable building products and energy conscious site planning techniques are strongly encouraged and shall be expected by the PC-ARC.



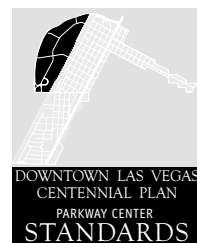
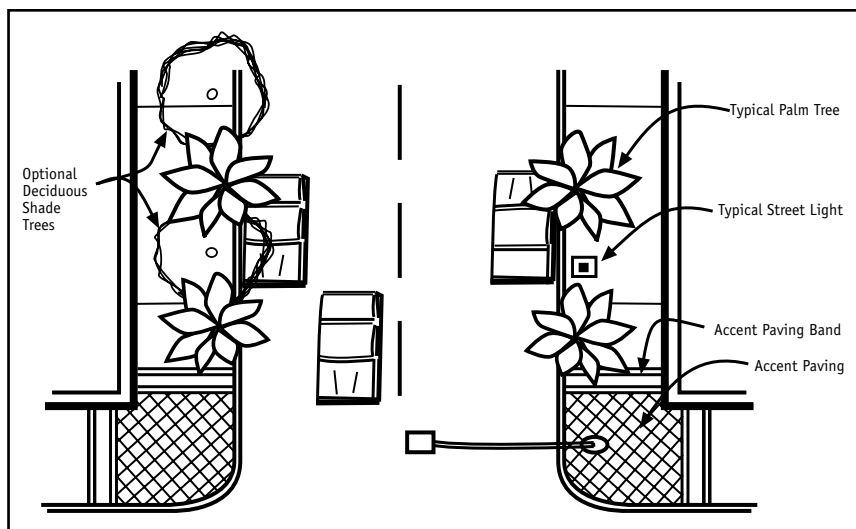
### 3. PEDESTRIAN ORIENTATION / STREETSCAPE

- 3.1. The design of all streets lights, benches, trash receptacles and other streetscape elements shall fit together with an overall concept of the planned development, and shall coordinate with such items specified within the Downtown Centennial Plan. Each element shall be coordinated with the other elements in size, shape, color and style.
- 3.2. North-South Streets shall be designed thematically using palm trees as their primary landscape element. Tree spacing should not exceed twenty-five (25) feet.

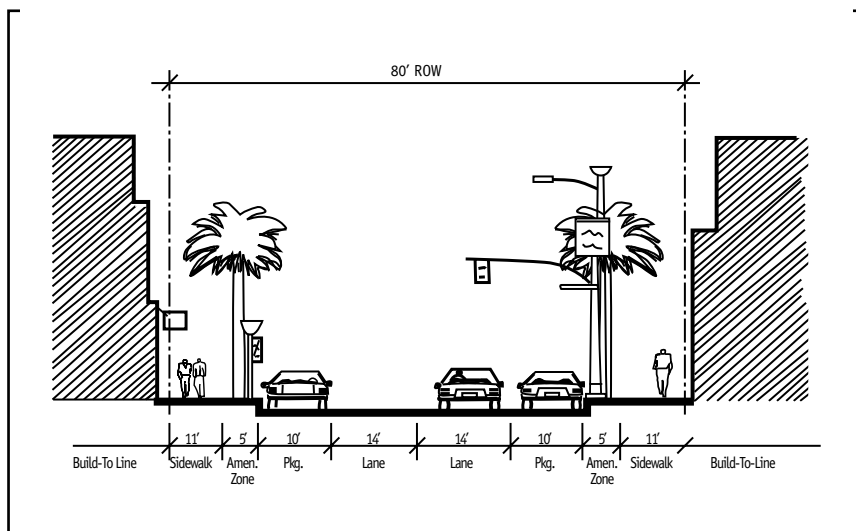
3.3. East-West streets shall use primarily shade trees at regular spacing not to exceed 30 feet.

Pedestrian pathways crossing streets shall be strongly delineated through the use of textures and/or patterns, and shall occur only at the intersection of streets, and streets and designated service drives.

#### PLAN VIEW



5'

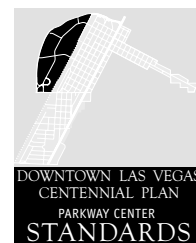
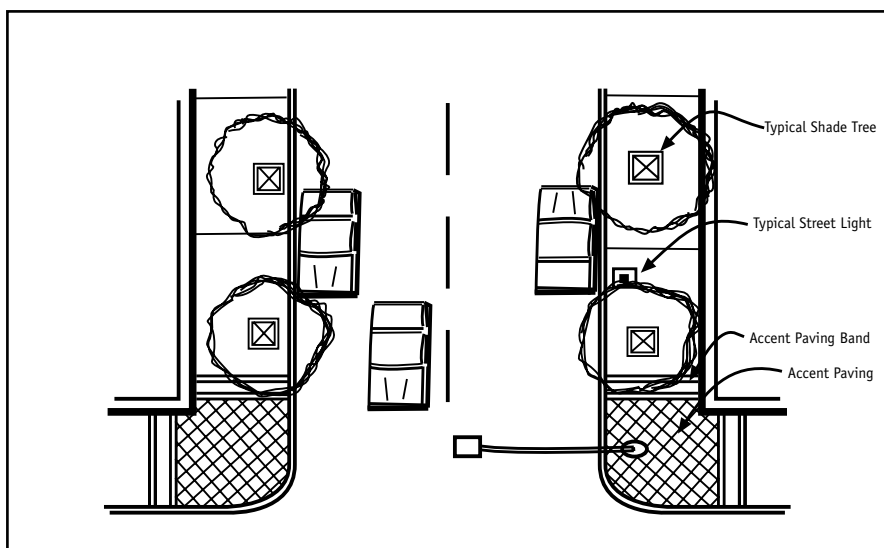


3.4. Pedestrian sidewalks along streets, and within the right-of-way, shall be delineated with a clear, rhythmic pattern and a texture to coordinate with those specified in the Downtown Centennial Plan or built within the Plan area during the recent past.

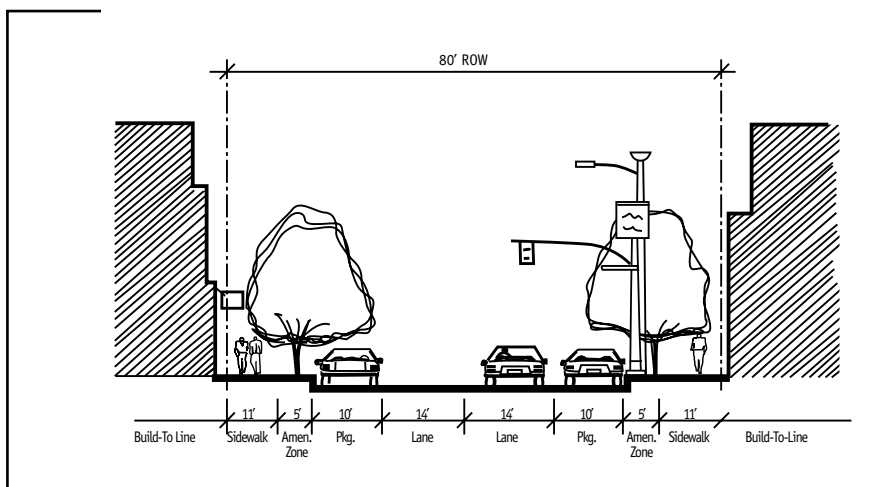
3.5. All street crossings shall be handicap accessible.

3.6. Street furniture shall match or coordinate with those specified in the Downtown Centennial Plan or built within the Plan area during the recent past.

PLAN VIEW



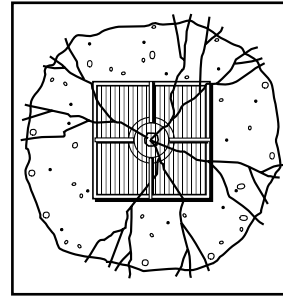
STREET SECTION



3.7. At least one litter container shall be placed with each bench or bench group.

3.8. Travel distances between benches along streets and primary pedestrian routes should

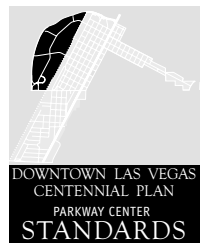
TYPICAL SITE FURNITURE



not exceed 400 feet, and there shall be a minimum of one bench area per parcel.

3.9. Wherever possible, seating groups shall be placed in shaded areas, and appropriately landscaped to reduce traffic noise and provide a pleasant atmosphere for conversation and socializing.

3.10. Street trees shall have installed planting grates of a similar design to those specified in the Downtown Centennial Plan or built within the Plan area during the recent past. Placement should be coordinated with landscape guidelines and surrounding paving patterns.



## 4. PARKING, TRANSPORTATION AND VEHICULAR ACCESS

4.1. The City shall work cooperatively with the Regional Transportation Commission and the Nevada Department of Transportation to develop a Parkway Center area wide master transportation plan. Such plan shall demonstrate the future development of the interstate highway systems including new and proposed on-ramps and off-ramps that will provide greatly enhanced access to Parkway Center. The plan shall also address internal circulation systems, and new needed vehicular access routes between Parkway Center and the downtown core districts, Charleston Boulevard, Martin Luther King Drive and Bonanza Boulevard.

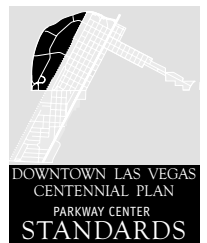
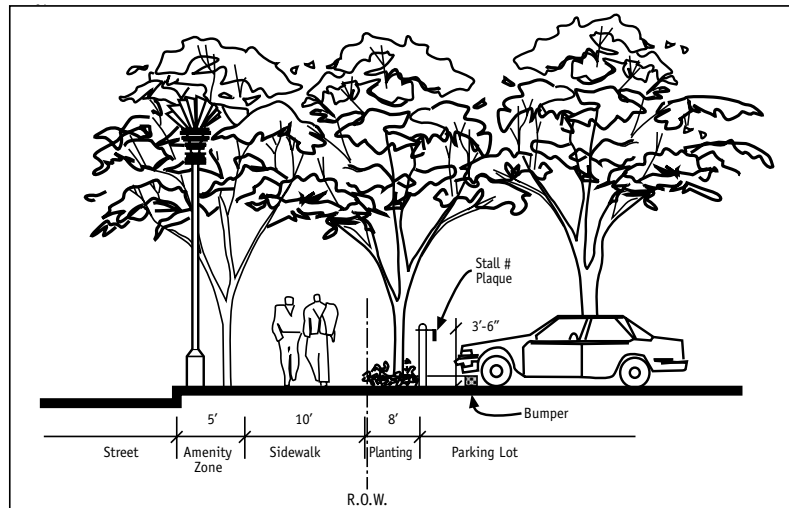
4.2. No on-street parking shall be allowed.

4.3. All off-street surface parking lots shall be paved and include access aisles and driveways. Surface treatments including paving and striping shall be maintained in good condition at all times.



4.4. Parking areas shall be separated from buildings through surface treatment variations, such as landscape buffers, and/or by grade separation. Parking spaces that directly abut buildings shall be prohibited.

4.5. Wherever practical, adjacent parcels each requiring access drives shall share a common, centrally located access drive.



4.6. Parking lots facing streets, primary pedestrian routes, or plazas and open space shall incorporate a minimum eight (8) foot landscaped setback, with 30 inch high berm or 42 inch high decorative fence to the parking lot side, along the entire length of visual exposure.

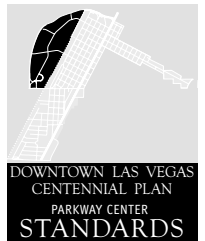
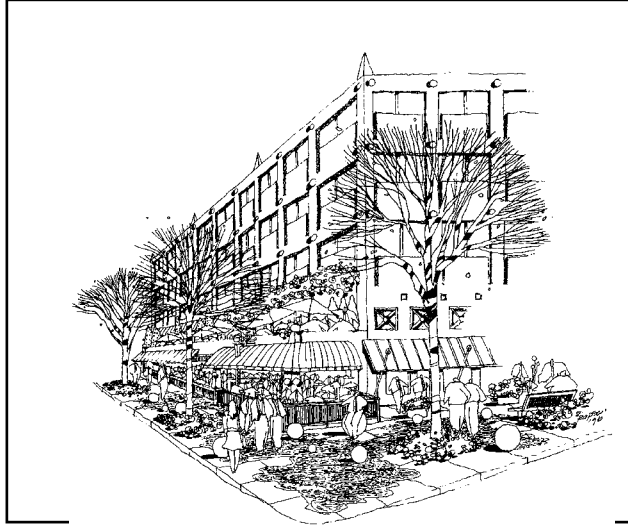
4.7. Surface parking lots shall have a minimum of one 24-inch box shade tree for every six spaces. At least 50 percent of these should be planted in landscaped islands. A minimum of 10 square feet of landscaped surface area shall be provided for each surface parking space, incorporated into islands and/or perimeter landscape treatment. Landscaped earth and rock berms are encouraged to screen surface parking. Refer to Title 19A of the City codes for minimum City configuration and layout standards, including handicapped parking stalls.

4.8. Above grade parking structures shall be set back from streets a minimum of 10 feet, except where desirable ground floor uses such as restaurants, retail, and commercial facilities are incorporated into the first floor of the structure facing the street. In this case, the setback is not required and appropriate build-to zones shall apply. The 10 foot required setback shall have sufficient landscaping to screen the structure at the pedestrian level, thus providing a visual amenity.

4.9. Bumpers or tire stops shall be provided in all parking stalls heading into a sidewalk, planting area, street or alley. They shall be installed at a minimum of two feet from the beginning of the sidewalk, planting area, street, or alley in order to effectively protect pedestrians, walkways, landscaping and signage.

4.10. Exterior walls of all parking structures shall be designed to be compatible with the

PARKING STRUCTURE WITH ARCADE AND CAFES



color, texture detail, and articulation of the main building or surrounding buildings. The intent is to minimize a garage appearance through designs that conform to the accompanying development's general architectural features. All interior surfaces of all parking structures shall be painted with a complimentary color with a minimum light reflectance value (LRV) of 0.75.

4.11. Bus drop-off areas shall be provided in close proximity to the entrances of newly constructed hotels and major retail facilities. Parking for buses shall be accommodated off-site or in a remote, non-obtrusive location so as not to interfere with normal traffic flow or pedestrian activity to and from the hotel or retail site.

4.12. The following minimum parking standards shall be provided on site for all development proposals. These standards shall provide minimally adequate parking but shall also encourage the use of alternative forms of mass transportation:

4.12.1.	Office	3.0 cars per 1,000 square feet
4.12.2.	Hotel	1.0 cars for every 3 hotel rooms
4.12.3.	Residential	1.5 cars per dwelling unit
4.12.4.	Retail	2.0 cars per 1,000 square feet
4.12.5.	Public Assembly	1.0 cars for every 10 seats
4.12.6.	Conference Center	0.5 cars per 1,000 square feet

5.1. The exterior materials of all buildings shall contribute to a cohesive physical environment and shall convey a sense of permanence and continuity. The intent of this standard is not to restrict creative expression or innovation, but rather to establish essential minimum standards.

5.2. Materials and colors shall be within the recommended palette of pre-selected materials and colors as may be determined by the PC-ARC in the future.

5.3. Permitted Exterior Building Finish Materials: The use of the following exterior building material finishes shall be permitted.

5.3.1. Stone

5.3.2. Synthetic or Simulated Stone Products, subject to special PC-ARC review

5.3.3. Brick, Block, or Terra Cotta Masonry

5.3.4. Finished Metal Systems

5.3.5. Glass

5.3.6. Concrete Masonry Units (CMU), of appropriate texture and color

5.3.7. Panelized E.I.F.S. (Exterior Insulating and Finishing System)

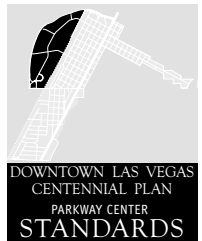
5.3.8. Tilt Up Concrete Panel Systems, but only on secondary and flex space buildings, subject to special PC-ARC review

5.4. Reflective or tinted glass shall not exceed 60 percent of the overall exterior enclosure of any building. Reflectivity of any glass shall not exceed 15 percent reflectivity index. Only non-reflective clear glass or non-reflective tinted glass with a visible light transmittance of above 60 percent shall be used on ground floors in all pedestrian-oriented areas. Final PC-ARC approval of glass shall be required.

5.5. E.I.F.S. (Exterior Insulating and Finishing System) materials shall not be used on exterior wall surfaces below 15 feet of finished ground level floor or below the second floor windowsill height, whichever is greater, subject to PC-ARC review and approval.

5.6. Samples of all primary exterior materials shall be submitted to the PC-ARC for approval, pursuant to Title 19A of the City codes standards for Site Development Plan Reviews.

5.7. Usage of Stone: Stone materials shall be used at a minimum as follows:



5.7.1. One (1) through five (5) story buildings: Stone cladding or stone and glass system shall be used to a height of not less than 15 feet above finished ground level floor or up to the second story window sill height, whichever is greater.

5.7.2. Buildings taller than five (5) stories: Stone cladding or stone and glass system shall be used to a height of not less than two (2) feet below the third story windowsill.

5.7.3. This standard shall not preclude the use of more stone than the minimum.

5.8. The use of high performance glass is encouraged.

5.9. Non-building surfaces such as retaining walls, planters, sunshades, arcades, canopies, etc., shall be finished to be compatible with the materials and colors of adjacent buildings.

5.10. The use of an articulated roofline or cornice shall be required.

5.11. Prohibited Exterior Building Finish Materials: The use of the following exterior building material finishes, as the main finish material or where visible from the street or any primary pedestrian routes, shall be prohibited.

5.11.1. Mirrored glass or dark tinted glass at the ground level

5.11.2. Wood and asphalt shingles

5.11.3. Wood or simulated wood products

5.11.4. Corrugated fiberglass or sheet metal products

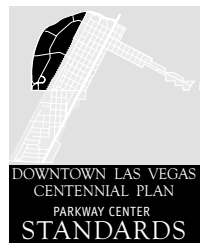
5.11.5. Stucco

5.11.6. Plastic laminates

5.11.7. Ceramic tile

5.12. Moderate use of decorative metal fencing is allowed provided that it shall coordinate with the color, and architecture of neighboring buildings. Property screening using wood or plastic materials is prohibited. Chain link fencing is prohibited except as temporary construction fencing in conjunction with obtaining a temporary construction permit.

5.13. Retaining walls, as necessary, shall be designed as an extension of the architectural character of the building. When abutting the building directly, retaining walls shall use the same architectural materials. When retaining walls are constructed as independent site elements, alternate surface materials, compatible with the adjacent development, may be proposed within the same color palette.



6.1. Landscape Architecture Concept for Parkway Center: The following is the landscape architecture concept for Parkway Center.

6.1.1. The objective of the landscape architecture design criteria is to establish a pleasant and attractive landscape framework for development. This framework shall provide design continuity and establish an identifiable visual character that enhances the community image and value of development, as determined by the PC-ARC.

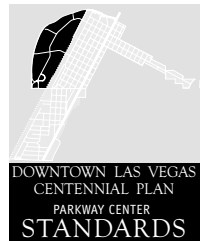
6.1.2. The overall landscape architecture concept is to use a cohesive palette of water conserving plants and landscape elements along all streets, primary pedestrian routes, and public open spaces. By contrast, more specialized landscape designs may be used on individual development projects, although some similarity in landscaping between parcels shall be required, as determined by the PC-ARC.

6.1.3. A gradation of plant materials shall be planned, progressing from low-maintenance, water conserving plants along streets and primary pedestrian routes, to more concentrated planting schemes with more lush plants and more intensive maintenance in public open spaces and near building entrances. Larger landscaped areas shall be predominantly low-maintenance, drought-tolerant materials. High-maintenance materials shall be concentrated in areas where pedestrians will frequently come into contact with them, such as at building entrances and public plazas.

6.2. Minimum Landscape Area: The minimum width of required landscape buffer areas per parcel shall be as follows:

- 6.2.1. Amenity zone next to street: 5 feet between curb and sidewalk
- 6.2.2. Berm next to street: 15 feet between curb and sidewalk
- 6.2.3. Primary pedestrian routes: 7 feet to each side of sidewalk, min. 6' wide
- 6.2.4. Front yard, if provided: 5 feet between sidewalk and building
- 6.2.5. Side yards, if provided: 10 feet between building and property line
- 6.2.6. Rear yards, if provided: 20 feet between building and property line

6.3. Plant Coverage: Trees are required in landscaped areas at the minimum rate of one (1) tree per 500 square feet of landscape area; however, tree spacing shall not exceed 30 feet. Trees provided in the Street Landscape Area do not count as part of this



requirement. The minimum plant sizes shall be as specified hereafter. Trees shall be a minimum of 24 inch box (with 36 inch box preferred) with 1-1/2 inch trunk diameter as measured 4-1/2 feet above the top of the box or root ball, shrubs shall be a minimum of 5 gallon pots, ground covers shall be a minimum of 1 gallon pots, and flowers or annuals shall be a minimum of 4" pots. The spacing of the plants shall not be under 2/3 of the plant's recommend mature growth.

6.4. Landscape Maintenance: All plant materials shall be maintained in good order, and shall receive acceptable levels of professional maintenance per industry standards, including irrigation controls, fertilizer, staking, and pruning, as necessary. Owners shall cause dead or dying plant materials to be replaced within 30 days, with such replacements to match the mature size and caliber of surrounding plant materials.

6.4.1. Cooperative Maintenance of Public Landscapes: Consideration shall be given to the creation of a landscape maintenance association, special improvement district, or some other organizational tool for the maintenance of common landscape and landscape within rights-of-way.

6.5. Landscape Plant List: The Las Vegas Valley Water District Desert Demonstrations Garden List of Plants shall serve as the Landscape Plant List for Parkway Center.

6.6. Plant Groupings: General plant groupings are trees, medium to tall shrubs, accent plants and groundcovers. Special considerations should be given when selecting plant material for irrigation compatibility, sun and shade exposure, frost tenderness and overall maintenance and care.

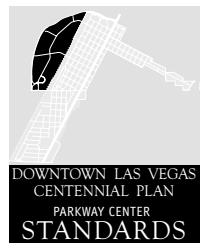
6.7. Landscape Screening: The intent of screening is to cost effectively minimize the impact of negative views on surrounding Owners and public areas.

6.7.1. Landscape Screening At Parking Areas: Landscape screening for Parking Areas shall meet the following requirements.

6.7.1.1. Consist of evergreen plants and be a minimum of four (4) feet high within 3 years of installation.

6.7.1.2. Shall be dense enough to provide screening of 75 percent upon maturity.

6.7.1.3. Trees shall be interspersed with shrubs and groundcovers at the rate of one tree every 20 lineal feet for the Street Landscape Areas.



6.7.1.4. Coordination shall be made so the new trees do not conflict with any existing streetscapes on adjacent properties.

6.7.2. Landscape Screening At Service Areas and Dock Areas: Landscape screening to be used in lieu of solid walls shall meet the following requirements:

6.7.2.1. Consist of evergreen plants and shall be a minimum of six (6) feet high within 3 years of installation.

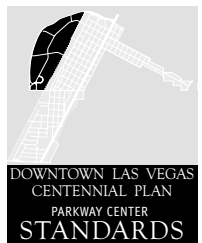
6.7.2.2. Shall provide adequate area and the plant selection shall be dense enough to provide screening of 75 percent upon maturity.

6.8. Street Trees: Trees shall be at a maximum spacing of 25 feet and shall be interspersed with shrubs.

6.8.1. North-south streets shall be designed thematically and shall use the Washington Mexicana Hybrid (Mexican Fan Palm). Space palms evenly and in regular intervals at 25 feet on center. The palms are to be 24 foot in height at initial time of planting and be with skinned trunks. All palms are to be evenly matched in height with matching trunk calipers. The palms are to be plumb and true when installed. All replacement palms are to match existing palms in height and caliber, maintaining uniformity of appearance along streetscapes. Drought tolerant and/or shade trees may be intermingled between palm trees if an occasional or continuous shade canopy is desired on north-south streets.

6.8.2. East-west streets shall be designed thematically with 24 inch box shade trees spaced a maximum of 30 feet on center (36 inch box preferred spaced 15 to 20 feet on center to create an immediate shade canopy) as the primary landscape element. A double row of shade trees is strongly encouraged along primary streets and primary pedestrian routes to provide a full shade cover to facilitate summertime pedestrian movement.

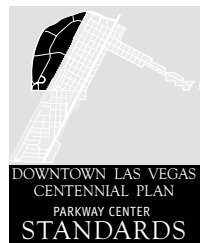
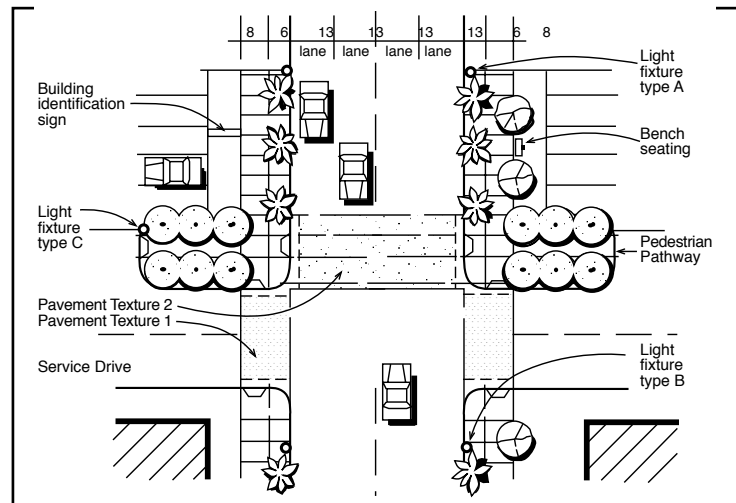
6.9. Sidewalk Cafe Zone: Sidewalk café zones may be designated by the PC-ARC in the future as deemed appropriate. Trees in such zones shall be deciduous with a higher canopy to provide easy pedestrian access in and around them. These trees shall be selected and arranged to provide summer shade and solar gain in the winter. The trees shall be spaced at a minimum distance of 25 feet on center and have a relationship with a streetscape, so as not to conflict with the existing or proposed streetscape.



6.10. Landscaping at intersection and entry drives: Accent trees shall be used at key entry drives to parcels and at all intersections. The trees shall be spaced at a maximum distance of 25 feet on center.

6.11. Parking Lot Trees: As a minimum standard, parking lot trees shall be installed as per Title 19A of the City codes. Additionally, parking lot trees shall be standard trunk trees that have an upright branching growth habit free from berries and harm-

TYPICAL STREETSCAPE ELEMENTS



ful branches or thorns.

6.12. Turf: The use of turf grass is allowed but shall not exceed 25 percent of the total landscape area of any development proposal.

6.13. Garden Spaces: Areas designated as parks, pocket parks, or garden spaces shall contain areas designated for public uses that provide art displays, plazas, water features, and seating available for small to moderate gatherings. Plant material coverage; trees, 1 tree per 500 square feet, shrubs and ground covers are to cover 75 percent of the designated landscape areas.

6.14. Gravel Mulch: Within Parkway Center, gravel mulch shall be used as specified herein.

6.14.1. Gravel Mulch for Street Landscape Areas: The purpose of the gravel mulch being used throughout the street landscape areas is to provide an attractive, uniform ground plane. The gravel shall complement the gray and green foliage of the planting providing a finished look to the landscape. It is also very effective in reducing wind blown dust and silty runoff. The gravel mulch specified shall be Pink Coral Sandstone, 1/2" to 3/4" screened material, installed and maintained at



a minimum depth of 2". (Source: Vegas Rock or equal approved by PC-ARC.)

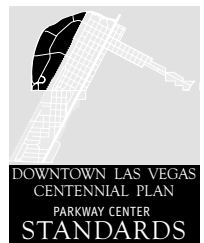
6.14.2. Gravel Mulch For Other Landscaped Areas: The purpose of gravel mulch being used in the landscape areas of the Lots is the same as in the Street Landscape Areas. In order to create an attractive and harmonious landscape throughout the business park, Owners are to use gravel mulch that is earth tone in color; the color should be harmonious with the gravel mulch used in the street landscape areas. The size of the gravel can vary to provide textural differences. Gravel mulch is required in all landscape areas. Gravel mulch is to be applied and maintained at a minimum depth of 2". A sample of the gravel mulch is to be included as part of the Material Sample Board during site development plan review.

6.15. Building Entries: Owners are encouraged to provide attractive landscaping at building entries. Specialty paving materials and site amenities are required at these areas and within adjacent Parking Areas, and subject to PC-ARC approval. Flowering trees and shrubs shall be used at building entries for accent color, as well as seasonal shade. Special accent paving of high quality materials are permitted to identify important pedestrian areas such as building entries, plazas and crosswalks. These shall be compatible with the overall color and material palette used for the architecture.

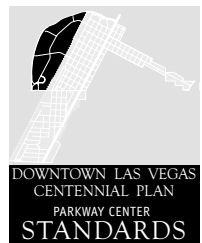
6.16. Site Amenities: Site amenities such as benches, bollards and planters are required at building entries or other pedestrian gathering areas. These are to be compatible with the overall color and material palette. Cut sheets and material samples are to be included as part of the material sample board during the final plan submittal.

6.17. Grading, Slopes and Drainage: Grading should result in a smooth contouring of parcels rather than harsh cut and fill slopes. Slopes shall not be greater than a 1:4 height to width ratio. Where retainage is required the retaining walls should not exceed 4 feet in height. Where additional retaining walls are needed, the walls are to be stepped with a minimum five (5) foot wide landscaped area between walls. Grading design shall ensure adequate site drainage that complies with City of Las Vegas standards.

6.18. Irrigation: Las Vegas Valley weather is characterized by low humidity, high winds and hot temperatures that can quickly desiccate all but the hardiest desert plants. The objective for irrigation design is to create water management systems that are water efficient and low maintenance. Automatic underground irrigation systems are required for all landscaped areas. Specific irrigation requirements are as follows:



- 6.18.1. Spray head-to-head coverage shall be required in all turf grass areas. Systems shall be designed so that peak summertime lawn irrigation can be completed between the hours of 7:00 P.M. and 12:00 A.M., and/or in compliance with the restrictions of the Las Vegas Valley Water Authority.
- 6.18.2. An electric, solid state controller is required and shall be equipped with a master valve terminal and at least two full independent programs.
- 6.18.3. All irrigated areas shall utilize remote electric control valves installed in valve boxes. No manual valves shall be allowed.
- 6.18.4. Drip irrigation shall be installed for all plant material of one gallon and larger size, within planting beds. Appropriate filtration and pressure regulating devices shall be installed. Low growing groundcover and annuals may receive pop-up spray irrigation. No fixed risers are allowed.
- 6.18.5. Paved surfaces including driveways within street landscape areas, shall be sleeved for pressure supply lines, non-pressure piping and control wires.
- 6.18.6. Spray heads shall not throw water onto walls, parking lots, sign faces, streets or sidewalks. Spray heads in groundcover areas shall be set back a minimum of 18 inches from back of curbs.
- 6.18.7. The irrigation controller time clock should be reset as necessary to reflect seasonal precipitation level and growth activity of the plant materials being irrigated.



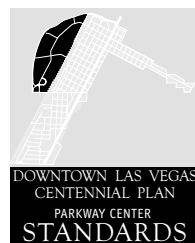
## 7. SIGNAGE DESIGN AND PLACEMENT

- 7.1. The purpose of establishing regulatory criteria for signs within this planned development is to reinforce the sense of a unique identity, create conformity between parcels, establish quality standards, reduce visual clutter, and promote sign legibility.
- 7.2. Colors and materials shall be consistent with the surrounding architectural materials.
- 7.3. Signs shall be easily read from the pedestrian level. Significant contrast between the background and letter should be used to contribute to the legibility of the information.

7.4. All freestanding signs shall be placed perpendicular to approaching vehicular traffic for easy visibility. They should be located so that vehicular sight distances around entry drives and intersections are not negatively affected.

7.5. Permitted Signs: Permitted sign types shall include the following:

- 7.5.1. Freestanding monument signs at entry points into the planned development
- 7.5.2. Guide signs within the planned development to enhance traffic circulation
- 7.5.3. Modular wall signs, including hanging signs and parapet signs no greater than 50 s.f. in size
- 7.5.4. Modular awning signs no greater than 25 square feet in size
- 7.5.5. Public utility signs
- 7.5.6. Animated signs
- 7.5.7. Neon signs
- 7.5.8. Letters and/or copy incorporated into the architectural structure of the building facade, parapet walls, or fascia as part of the architectural expression. The size, font, and character shall be determined by the PC-ARC.



7.6. Prohibited Signs: Prohibited sign types shall include the following:

- 7.6.1. Temporary A-frame signs
- 7.6.2. Beacon Lights
- 7.6.3. Bench Signs
- 7.6.4. Billboard Signs (other than existing interim billboards)
- 7.6.5. Changeable Copy signs
- 7.6.6. Tethered Balloons
- 7.6.7. General Advertising Signs
- 7.6.8. Magnetic signs
- 7.6.9. Portable Signs
- 7.6.10. Paper, cloth or plastic streamers
- 7.6.11. Painted signs on display windows with temporary paint
- 7.6.12. Pennants
- 7.6.13. Roof Signs
- 7.6.14. Statuary with Lettering or graphics
- 7.6.15. Projecting signs into the right of way of any street
- 7.6.16. Pole Signs
- 7.6.17. Painted Wall Signs

7.7. Existing interim billboards located upon any parcel within Parkway Center shall be addressed with each submission of a Site Development Plan Review.

7.8. Building Identification Signs: All office buildings shall be identified with identification signs oriented to streets and/or primary pedestrian routes. Identification signs shall clearly display the buildings street address at the top, and provide space for appropriate recognition of the building's primary tenants. Each identification sign shall be constructed similar in design and materials to the drawing provided below. Building identification signs shall be located on the property line along streets. There shall be no greater distance than 100 feet from the face of sign to the face of building.

7.9. Wall or building mounted signs: The following standards shall apply to all wall or building mounted signs.

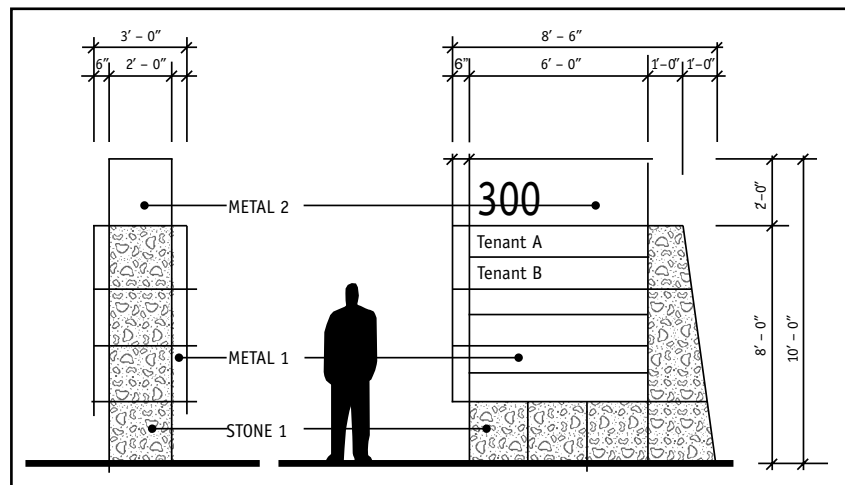
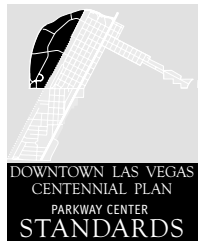
7.9.1. All signs shall fit within the architectural frame or sign band of the storefront, or occupy the same square footage to enhance the architectural character of the buildings.

7.9.2. The size of any sign shall be consistent with the proportions of scale of building elements in the facade.

7.9.3. All signs should establish a facade rhythm, scale and proportion where not currently existing.

7.9.4. All signs should be appropriate in color and texture to the building on which it is placed.

7.9.5. No building signs shall be permitted above the bottom of the second floor windowsill. Signs above this height may be allowed for a maximum of two



(2) tenants per office building or for the name of a hotel and are limited to letter signs, either illuminated or non-illuminated, with one (1) square box sign allowed in which a logo can be illuminated. The size, font, and character to be determined by the PC-ARC.

7.9.6. The identification of each building or store's address is required. A minimum of 6-inch high numbers should be used to identify the address. This should be located within ten (10) feet of the main entry, and in conformance with Chapter 28 of the Las Vegas Municipal Code.

## 8. LIGHTING

8.1. Streetlights, surface parking lights, pedestrian lights and decorative lights shall be incorporated into any proposed streetscape improvement. Streetlights shall provide the light needed for safety and traffic operations. Pedestrian lights shall be placed in a clear geometric pattern with regular spacing to provide a sense of order to the street and even light distribution.

8.2. Recommended street lamp poles and luminaires to be used throughout the development shall match new lights recently installed in the Office Core District as specified within the Downtown Centennial Plan. Pedestrian lights and bollards shall be coordinating styles and produced by the same manufacturer to provide a consistent street lighting hierarchy.

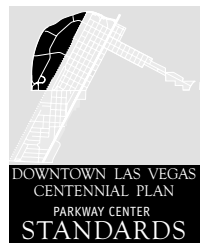
8.3. Building shall be sensitively lit at night through the use of wall mounted sconce fixtures or soffit-mounted fixtures at the lower two stories.

8.4. The upper floors may be lit from concealed sources provided that any uplights do not create light spillover to adjacent parcels and do not create skyshine.

8.4.1. The building top or crown, may be accented through lighting from concealed sources.

8.4.2. Neon lighting (except when used as signage), exposed bulbs, and wall mounted arm lights are prohibited, except on a case-by-case basis within the Special Signage Sub-district of the Downtown Casino Overlay District.

8.4.3. Lamps and ballasts shall be a Venture Uniform pulse start system. Parking

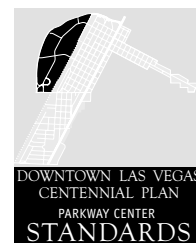


and roadway fixtures shall use 320 watt, metal halide, 40000 K, 65 CR1 lamps. Walkway poles and bollards shall use 100 watt, metal halide, 40000 K, 65 CR1 lamps.

8.5. Minimum lighting levels shall be as follows:

8.5.1. Parking Lots: Lighting levels in parking lots shall be 0.5 Foot Candles (FC) minimum, and with 10:1 uniformity.

8.5.2. Parking Structures: Lighting levels in parking structures shall be 5.0 FC minimum in general; 10.0 FC minimum on ramps; 50.0 FC minimum at entrances and exits, and with 4:1 uniformity.



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*Parkway Center Proposed Development Standards (Draft: 01/02/02)*